

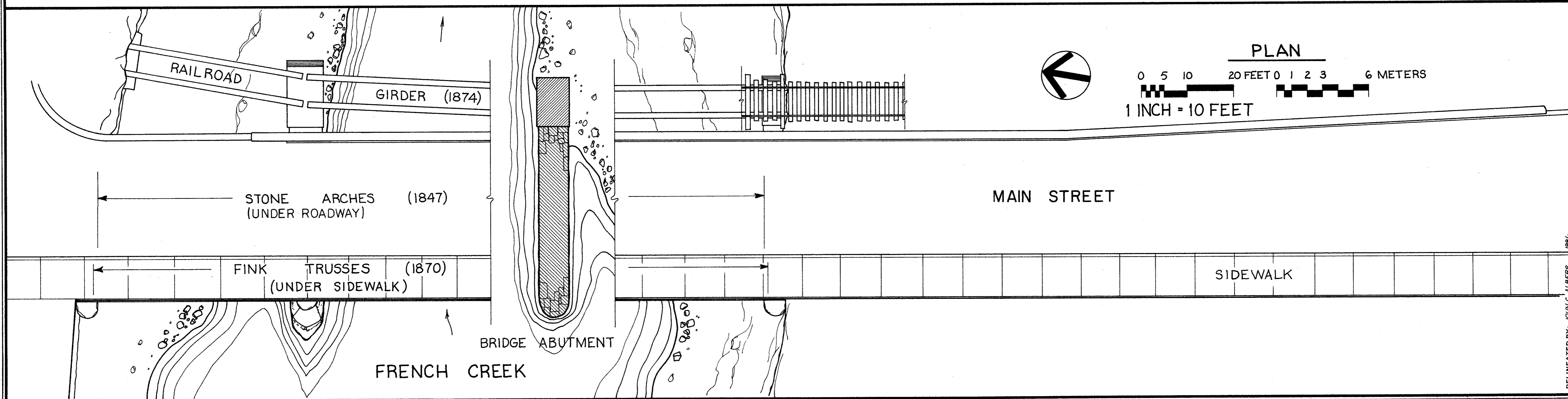
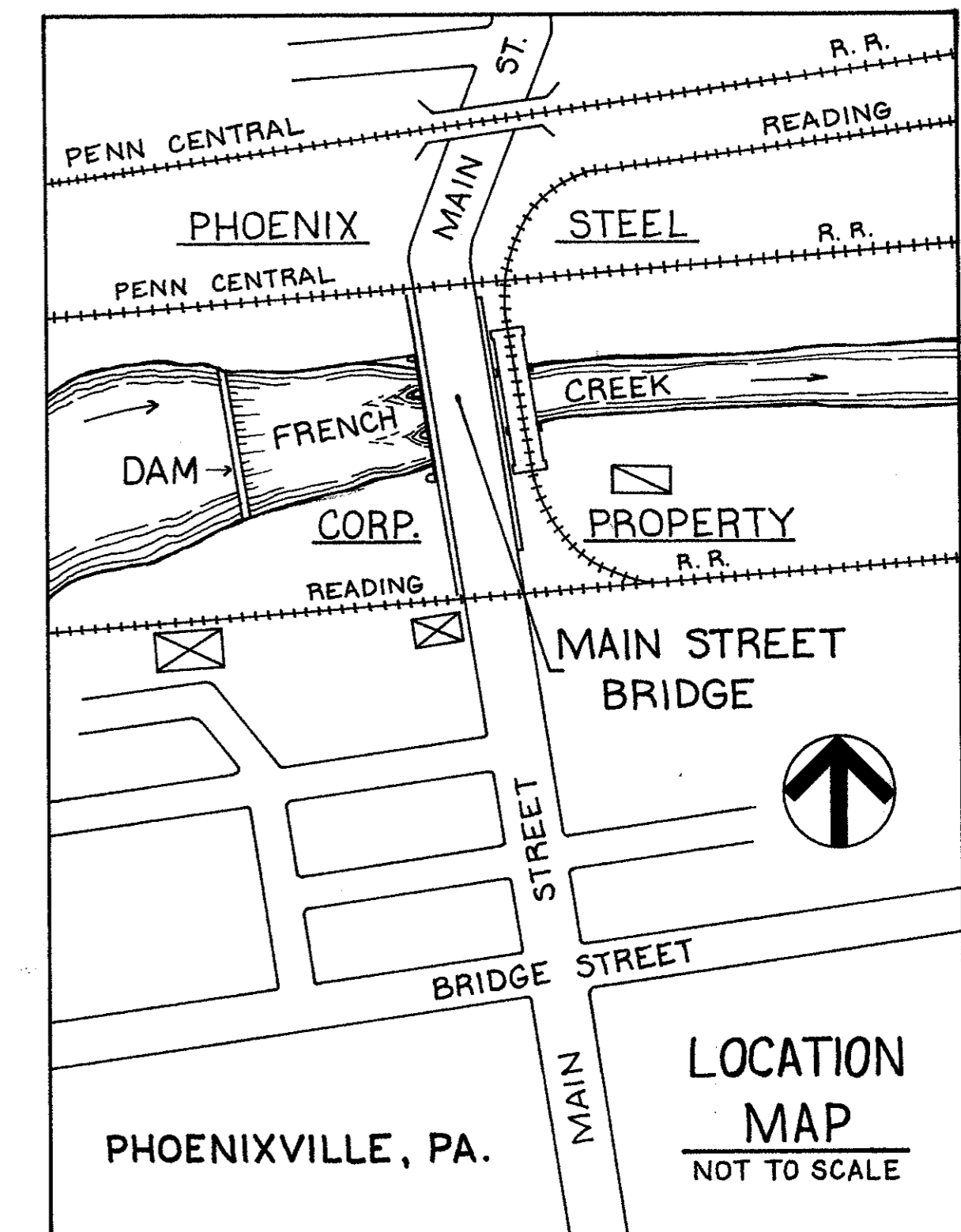
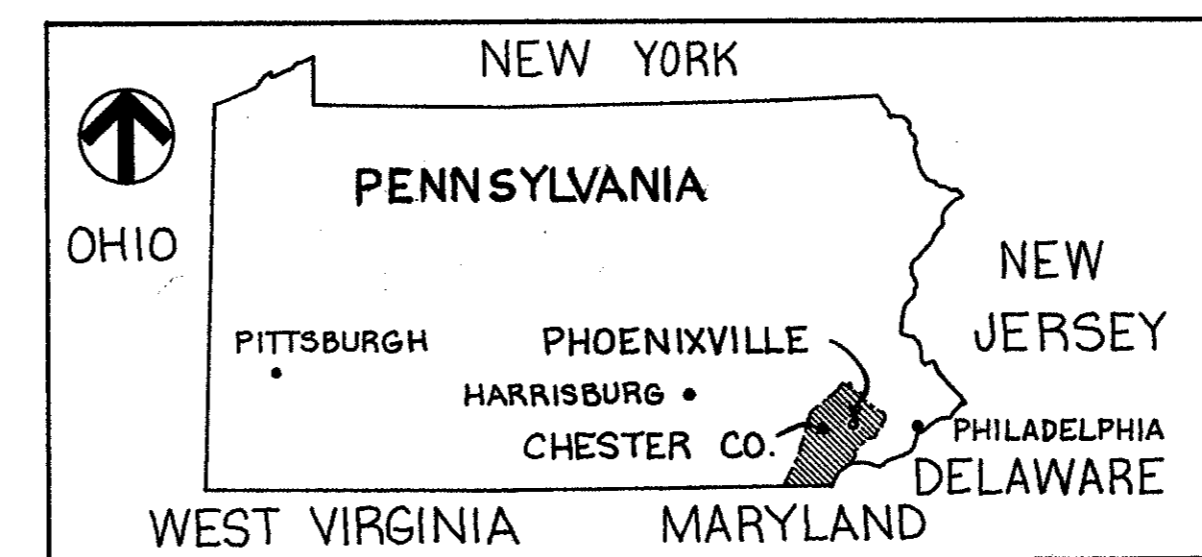
MAIN STREET BRIDGE - 1847, 1870, 1874

PHOENIXVILLE, PENNSYLVANIA

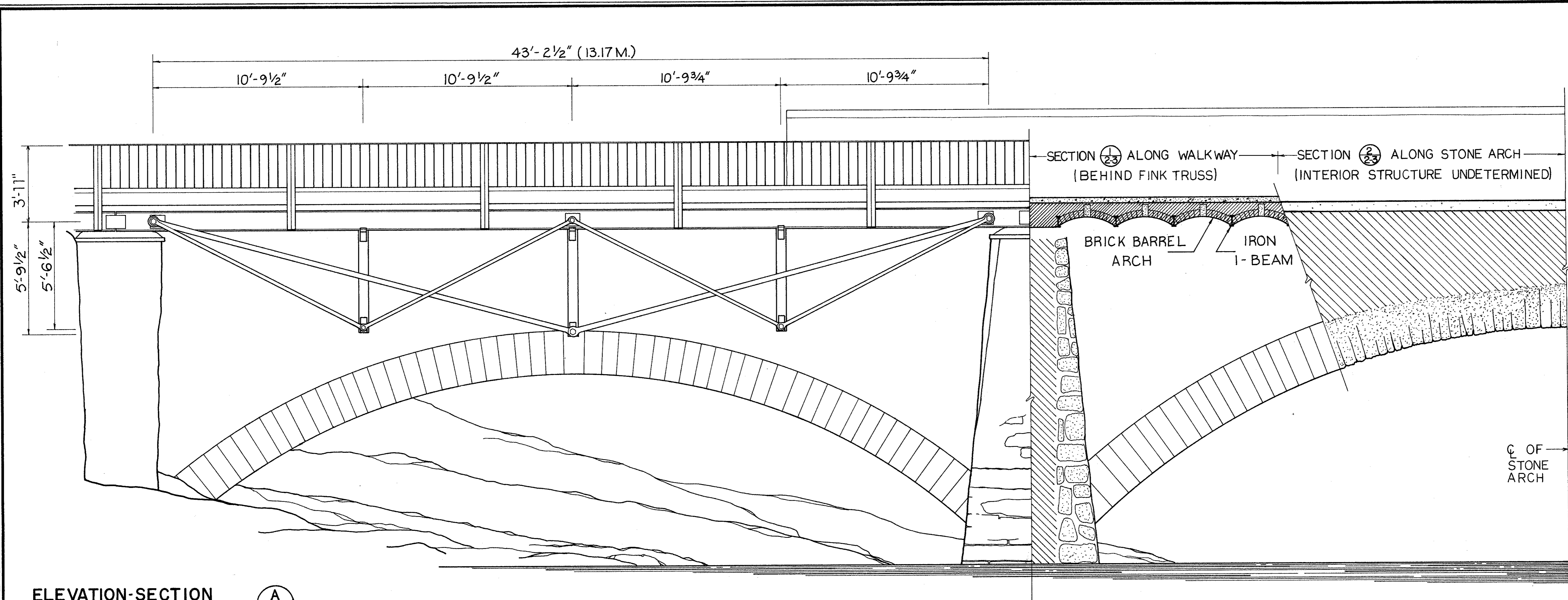
THE MAIN STREET BRIDGE IN PHOENIXVILLE, PENNSYLVANIA IS A THREE SPAN COMPOSITE, MULTI-MODAL STRUCTURE LOCATED ADJACENT TO THE INDUSTRIAL COMPLEX OF THE HISTORIC PHOENIX IRON COMPANY. THE BRIDGE CONSISTS OF THREE ADJOINING COMPONENTS WHICH WERE CONSTRUCTED INDEPENDENTLY TO ACCOMODATE THE CHANGING NEEDS OF THE TOWN AND ITS INDUSTRIAL BASE. FIRST, A THREE-SPAN STONE ARCH BRIDGE WAS BUILT IN 1847 FOR VEHICULAR TRAFFIC. A PEDESTRIAN PORTION, CONSISTING OF THREE FINK DECK TRUSSES, WAS ADDED TO THE WESTERN SIDE OF THE STONE STRUCTURE IN 1870. IN 1874 THE STONE PIERS OF THE ORIGINAL ARCH BRIDGE WERE EXTENDED ON THE EASTERN SIDE IN BRICK, AND A PLATE GIRDER RAILROAD BRIDGE WAS BUILT TO CONNECT THE PARTS OF THE PHOENIX IRON COMPANY COMPLEX LOCATED ON BOTH SIDES OF FRENCH CREEK.

THE MASONRY PORTION OF THIS BRIDGE, AS ORIGINALLY CONSTRUCTED, IS REPRESENTATIVE OF THE MULTIPLE SPAN STONE BRIDGES BUILT THROUGHOUT PENNSYLVANIA IN THE MID-NINETEENTH CENTURY. IT HAS BEEN REPAIRED FREQUENTLY AND PORTIONS HAVE BEEN REBUILT FROM ITS EARLIEST YEARS; MOST OF THE ORIGINAL STRUCTURE IS NOW OBSCURED BY CEMENT PARGING. THE MOST SIGNIFICANT COMPONENT OF THIS BRIDGE IS THE PEDESTRIAN IRON FINK TRUSS PORTION WHICH WAS CONSTRUCTED IN 1870. ALBERT FINK, PROMINENT NINETEENTH CENTURY ENGINEER, PATENTED THIS TRUSS TYPE IN 1854. ONE OF TWO KNOWN FINK DECK TRUSS BRIDGES REMAINING IN THE UNITED STATES, THESE ARE THE ONLY KNOWN EXTANT FINK TRUSSES IN PENNSYLVANIA. THE TRUSS COMPONENTS AND THE IRON RAILING WERE ALL FABRICATED AT THE PHOENIX BRIDGE WORKS, AND THEY WERE ERECTED BY THAT COMPANY.

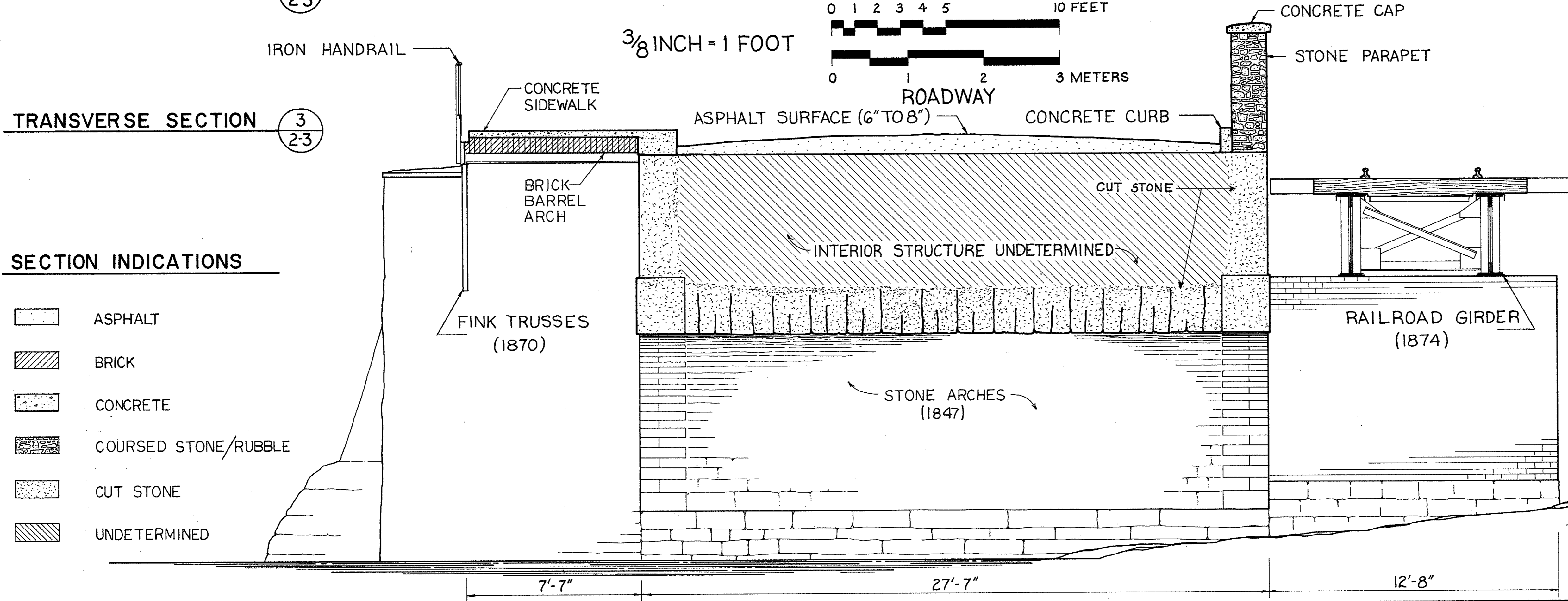
THE MAIN STREET BRIDGE WAS RECORDED BY THE FIRM OF PAULA A. C. SPERO, HISTORIC STRUCTURES CONSULTANT, BALTIMORE, MARYLAND FOR CHESTER COUNTY, PENNSYLVANIA. THIS DOCUMENTATION WAS COMPLETED TO FULFILL THE OBLIGATIONS OF THE COUNTY OF CHESTER, THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION AND THE FEDERAL HIGHWAY ADMINISTRATION AS SPECIFIED BY A MEMORANDUM OF AGREEMENT BETWEEN THEM, THE PENNSYLVANIA STATE HISTORIC PRESERVATION OFFICE AND THE ADVISORY COUNCIL ON HISTORIC PRESERVATION, PURSUANT TO 36 CFR 800. THE PROJECT TEAM CONSISTED OF PAULA A. C. SPERO, ENGINEER AND HISTORIAN; JOHN G. ALBERS, ARCHITECT AND DELINEATOR; AND ROBERT C. SHELLEY, PHOTOGRAPHER.



HISTORIC AMERICAN ENGINEERING RECORD
 SHEET 1 OF 3
 PENNSYLVANIA
 CHESTER COUNTY
 PHOENIXVILLE
 MAIN STREET BRIDGE - 1847, 1870, 1874 - SPANNING FRENCH CREEK
 PA-84
 DELINEATED BY: JOHN G. ALBERS 1984
 HISTORIC AMERICAN ENGINEERING RECORD
 UNITED STATES DEPARTMENT OF THE INTERIOR
 IF REPRODUCED, PLEASE CREDIT: HISTORIC AMERICAN ENGINEERING RECORD, NATIONAL PARK SERVICE, NAME OF DELINEATOR, DATE OF THE DRAWING



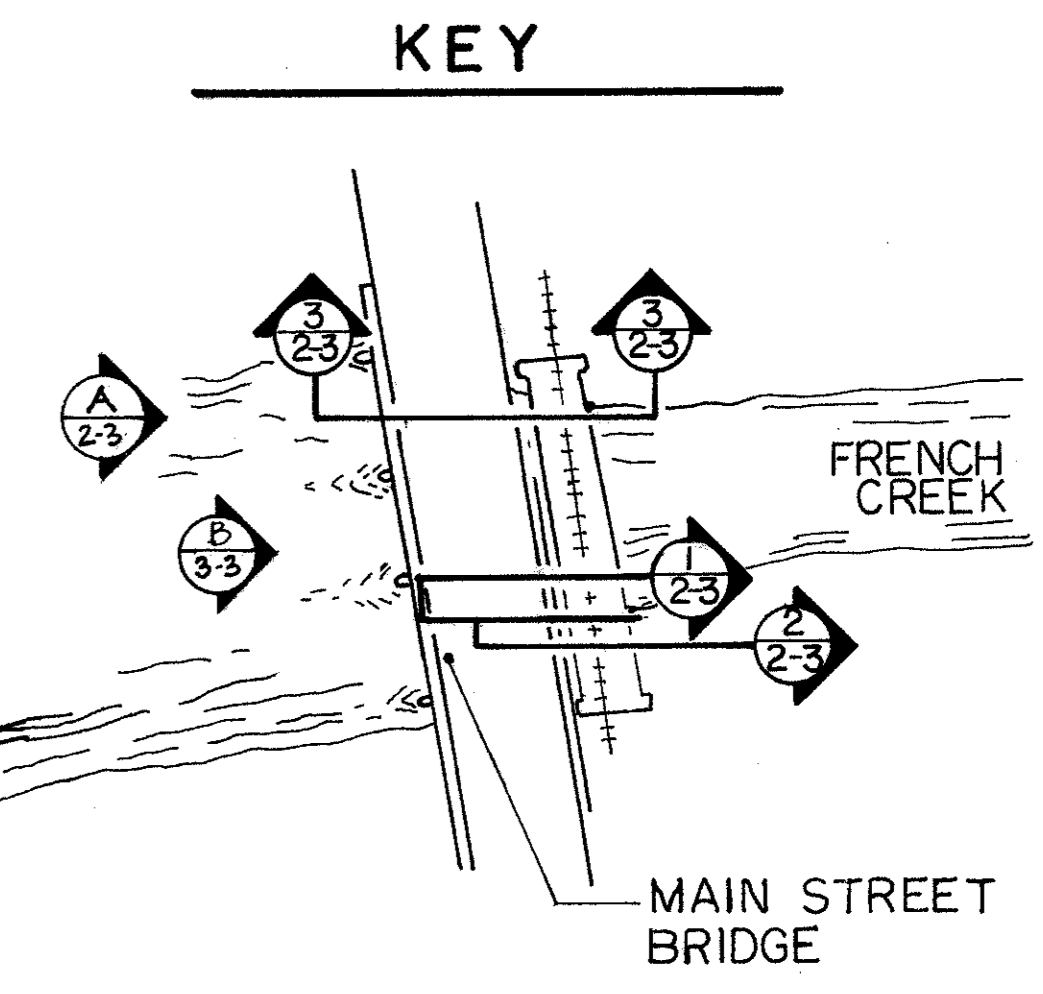
ELEVATION-SECTION A
2-3



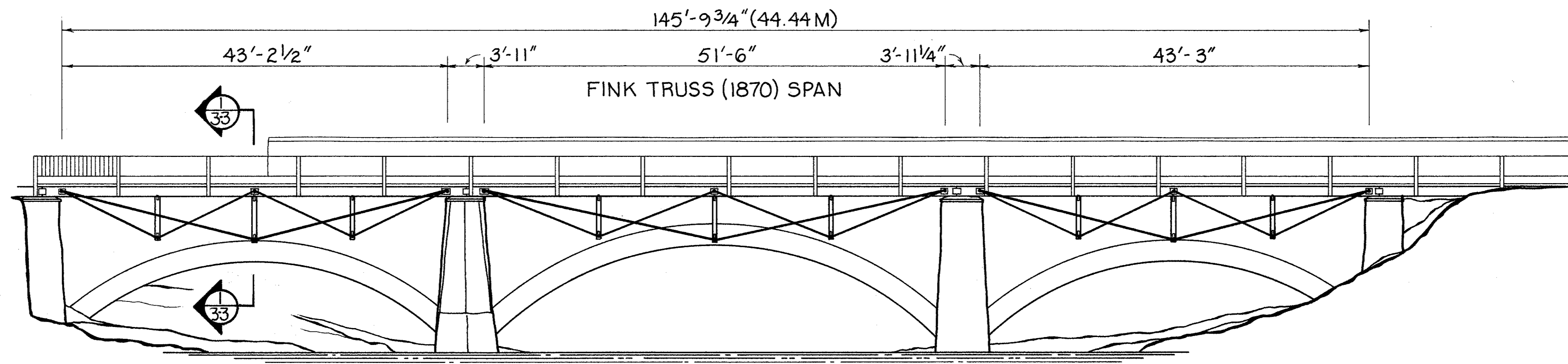
TRANSVERSE SECTION 3
2-3

SECTION INDICATIONS

- ASPHALT
- BRICK
- CONCRETE
- COURSED STONE/RUBBLE
- CUT STONE
- UNDETERMINED



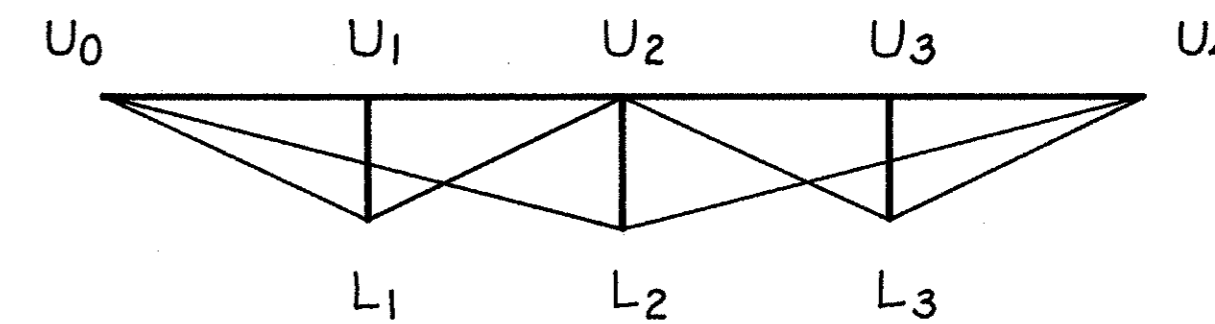
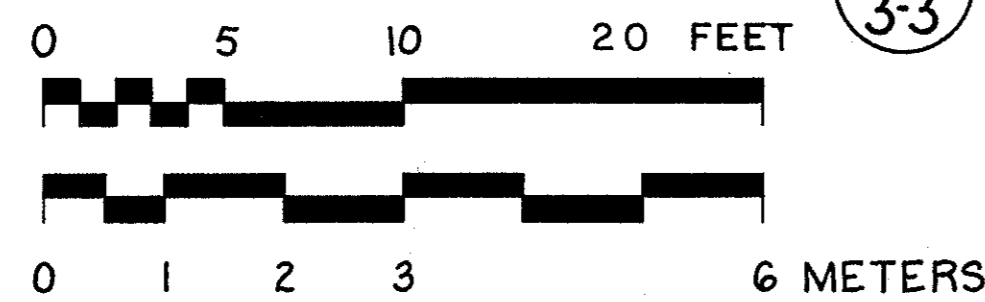
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 MAIN STREET BRIDGE-1847, 1870, 1874, - SPANNING FRENCH CREEK
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 SHEET 2 of 3
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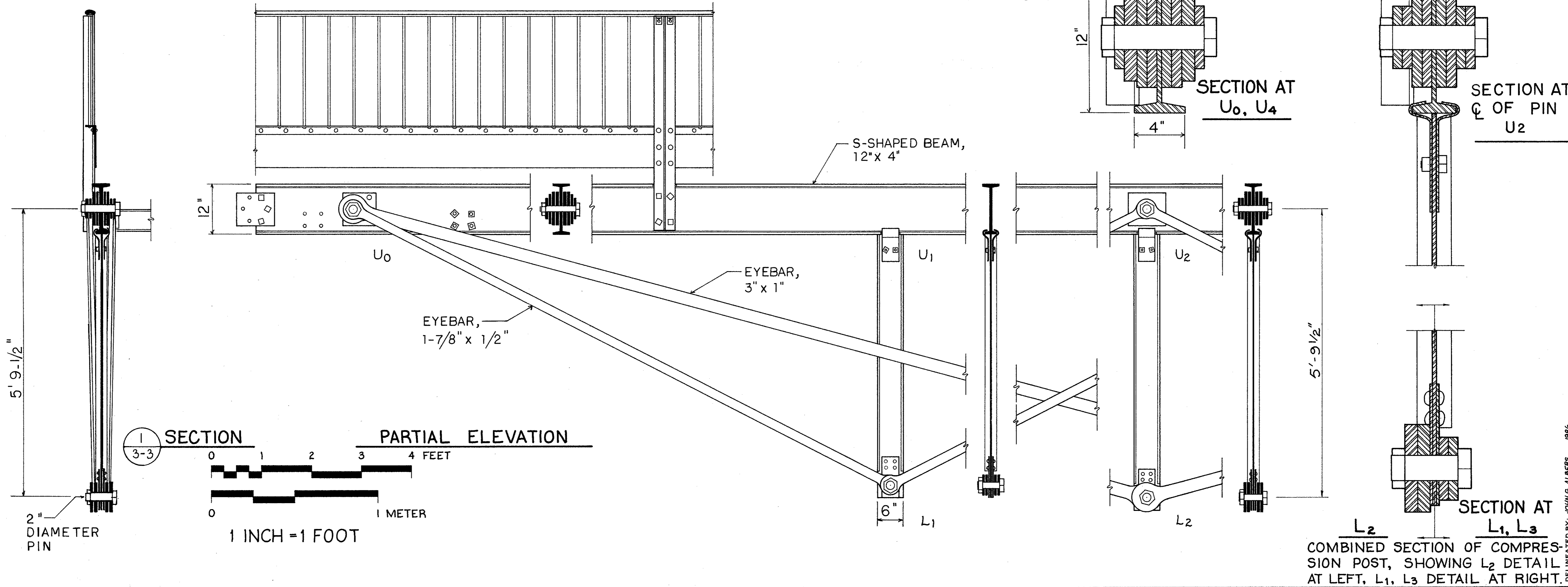
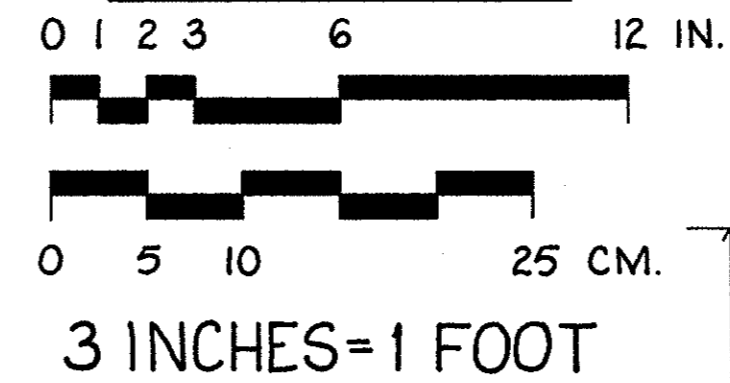
UPSTREAM (WEST) ELEVATION

B
3-3

3/16 INCH = 1 FOOT



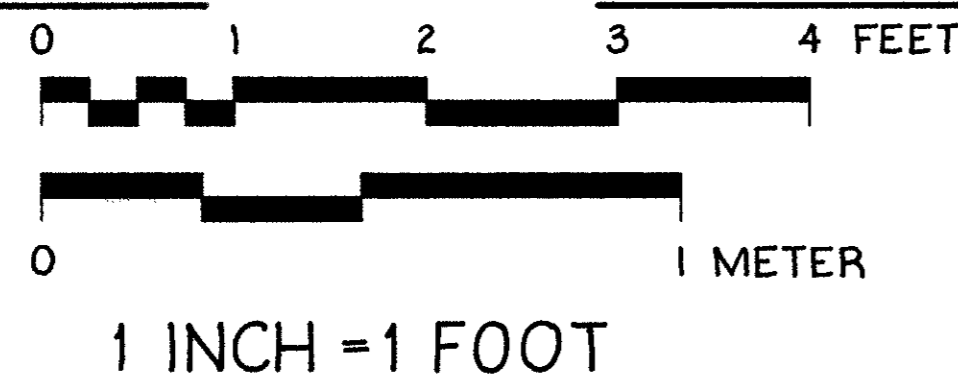
DETAILS



I SECTION

3-3

PARTIAL ELEVATION



HISTORIC AMERICAN ENGINEERING RECORD PA-84
 SHEET 3 OF 3
 PENNSYLVANIA
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