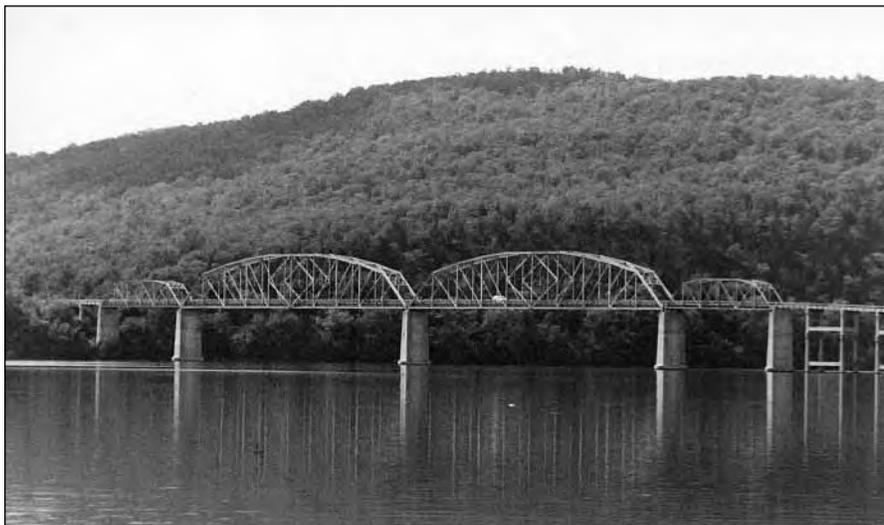
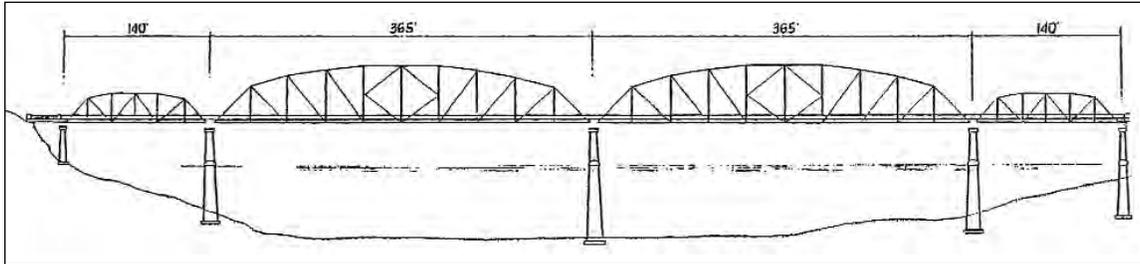


(#129) 58-SR002-21.19: Marion County Memorial Bridge spanning the Tennessee River southeast of Jasper in south-central Marion County (Sequatchie Quad, 100 SE).



Significant under Criterion A as a toll bridge and Criterion C as representative Parker and Warren trusses by the state highway department.

The state legislature designated this site, which a ferry then serviced, for one of its toll bridges in 1927. This crossing was a key link in the Wauhatchie Route of the Dixie Highway between Chattanooga and Jasper. The state highway department designed the bridge and built it for \$442,287.93 primarily between April 1929 and November 1930. State crews built a toll house for \$652.17 between July and November 1931. The total project cost of \$488,848.68 included core drilling, grading, and surfacing (Johnson 1978a:163-168; Keeble 1947-1-8; Raulston and Livingood 1974:200-207; Tennessee 1931:88-95; 1933:106-115; 1943:119; TVA 1972).

Soon after the state completed the bridge, between 1930 and 1932, it removed the U.S. 41 designation from the Suck Creek Route of the Dixie Highway, State Route 27, and applied it to this route, State Route 2, from Jasper to Chattanooga. The state then upgraded State Route 2 from Jasper to Chattanooga and renamed the road the Will Cummings Highway. As part of U.S. 41, this bridge was on the main north-south corridor in the area, and revenue was sufficient to keep the bridge open as a toll bridge until 1947.

The bridge was located near the 1905-1914 Hale's Dam, which consistently had leakage and other problems primarily due to foundation problems. Repairs were repeatedly made to the dam, including some by TVA that acquired it in the 1930s. By the 1960s TVA deemed it more

economical to erect a new dam than try to continue repairing the old one. As a result, TVA built the Nickajack Dam in the 1960s downstream from this bridge crossing. Due to changes in water levels, TVA rebuilt this bridge. In order to provide a 57-foot vertical navigation clearance, TVA placed caps on top of the original piers and raised the main truss spans 21 feet. Twenty pre-cast, pre-stressed concrete slabs replaced the original 28 concrete approach spans, eliminating nine approach spans on the east end and adding one on the west end. Although TVA built one new bent, it reused most of the original bents by adding caps. Other repairs included new abutments and a new handrail to replace the original lattice railing. Although some initial work was done in 1966, the bridge was closed from December 1967 to December 1968 while the majority of the work was done.

The bridge contains four riveted through truss spans. The main two spans are 365-foot Parkers. The other two trusses are 140-foot Warren trusses with polygonal top chords. The bridge has a curb-to-curb width of 20.0 feet, and an out-to-out width of 24.0 feet. Composition of the members is typical for this era. Top chords and end posts are channels with stay plates or lacing, bottom chords are channels with lacing on top and bottom, posts are I-beams, and diagonals are I-beams or laced channels.

The Tennessee Department of Transportation, in cooperation with the Federal Highway Administration, has scheduled this bridge for replacement.