

OVERVIEW

Bayou St. John Bridge Restoration Project The Dumaine St. and Magnolia Bridges



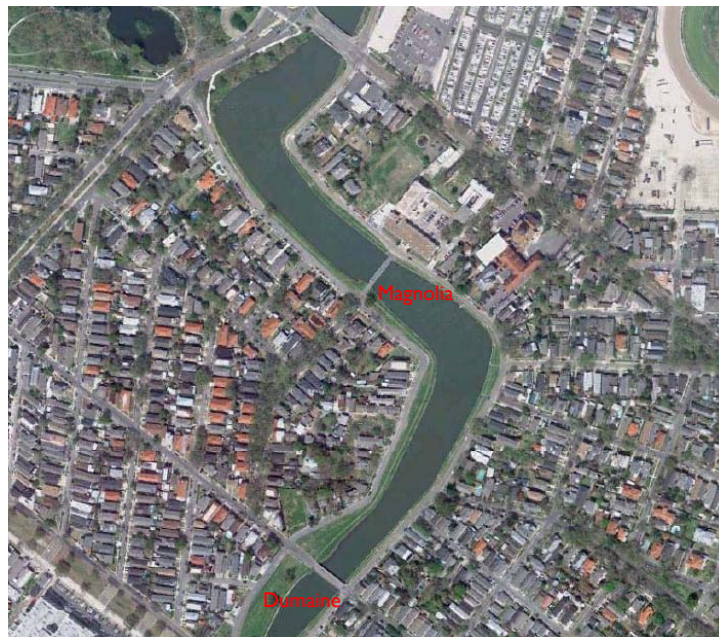
Presented by the
Faubourg St. John Neighborhood Association

Bayou St. John Bridge Restoration Project: The Dumaine Street and Magnolia Bridges



Faubourg St. John is a historic neighborhood in the heart of New Orleans, roughly bounded by North Broad St, Orleans Avenue, North Carrollton Ave, and the fabled Fairgrounds Race Track. Running through this neighborhood is Bayou St. John, a critical waterway in the establishment and early development of the City of New Orleans. Houses have been being built along the banks of Bayou St. John for 300 years, and as the neighborhood developed several bridges were erected. This project addresses two of those bridges: The Dumaine Street Bridge and the Magnolia Bridge, also referred to as the Cabrini Bridge.

The Dumaine Street Bridge is a sixty year old, fully functional, vehicular bridge that is in need of repair and beautification. The Magnolia Bridge is approximately 150 years old and has been closed to vehicular traffic for over thirty years. Since that time it has been heavily used by pedestrians, bicyclists, dog-walkers and baby-strollers, and has provided a beautiful backdrop for weddings and other festive gatherings. Structural repair work was last done on this bridge in 1961, and 50 years later there is a significant amount of corrosion and disrepair.



Intent

The Faubourg St. John Neighborhood Association is seeking to fully repair, restore, and beautify these two bridges in 2011. Our scope of work will address civil and structural needs equally with architectural restoration, guided by historic relevance and a green conscience, and will include a 20 year maintenance implementation plan and budget.

The Neighborhood Association has established a committee to drive this project and that committee is developing a strategy to fund and manage the work. The exact scope of work will be defined by a feasibility study that is currently underway. Based on initial assumptions, the committee has developed a preliminary budget and an outreach strategy to inform and include a diverse group of stakeholders regarding this project, including: neighboring schools and churches, area neighborhood associations, governmental agencies, elected officials, civic, environmental, and historic organizations, and educational institutions with relevant course offerings.

These two wonderful bridges frame the graceful curve of Bayou St. John and the historic portage route to the Mississippi River; the very reason why the City of New Orleans is located where it is. Along with the gleaming dome of Holy Rosary church and the historic architecture of barge-board built houses, this is one of our city's most iconic street and waterscapes. The restoration of these bridges will significantly enhance not only the public use and walkability of the area, but also residential and commercial property values, and the overall quality of life for residents and visitors alike.



For more information: bayoubridges@gmail.com

History – Magnolia Bridge

The Magnolia Bridge is an iron bridge built in the mid-19th century, and is perhaps the oldest surviving bridge in New Orleans. It was designed to rotate at its center to allow ships to pass.



This photograph of the Magnolia Bridge was taken some time between 1880 and 1920. It shows a bridge tender's structure on the west side of the bayou, at the foot of Harding St.

From the George Francois Mugnier Collection, archived at the Louisiana Digital Library

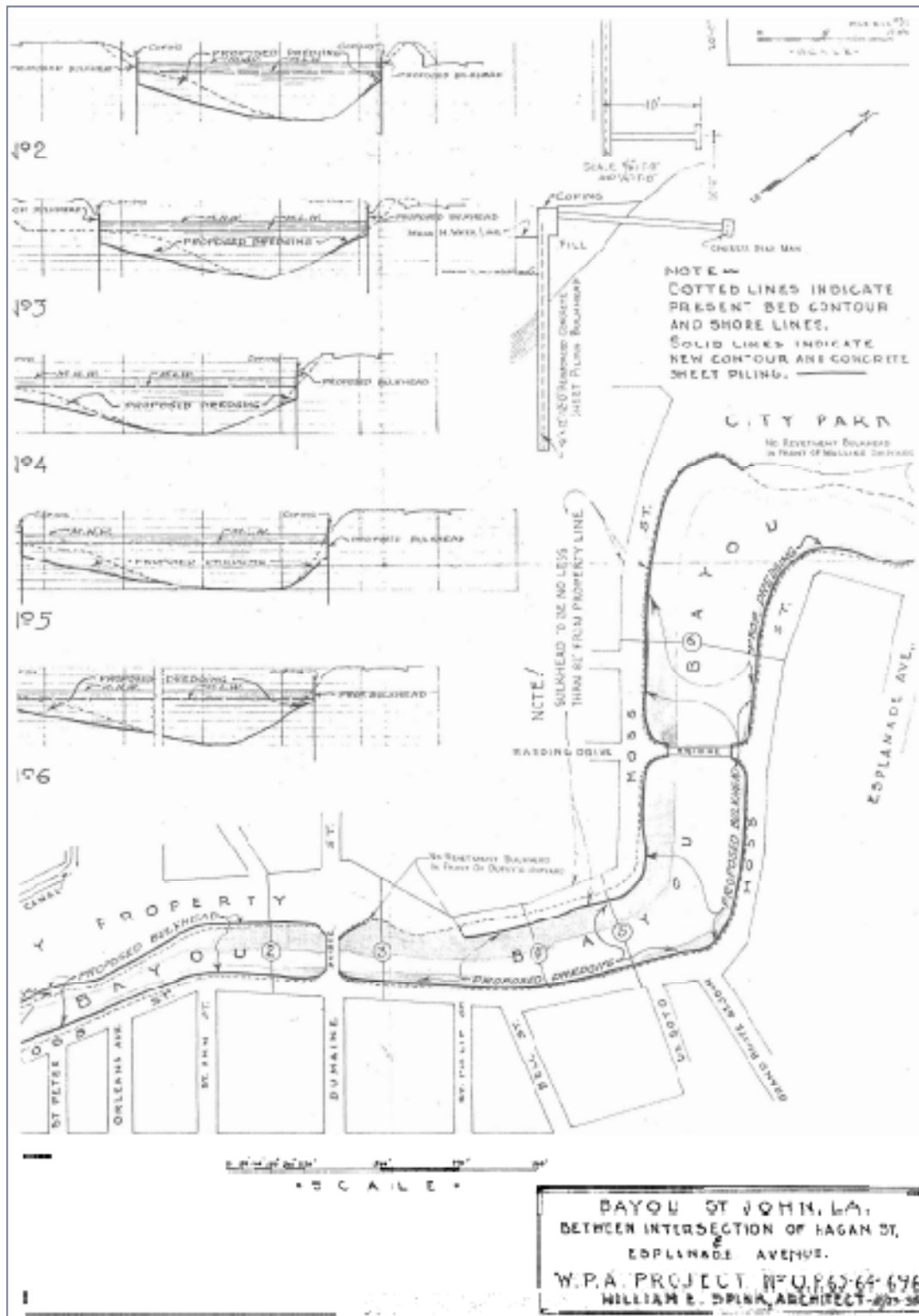


This photograph was most likely taken in the 1920s, when the bayou was still a navigable waterway. In 1936 the bridge was stabilized when Bayou St. John lost its status as a navigable waterway. It remained a vehicular bridge until the 1970s when balustrades were installed, to block motor vehicles.

From the Frank B. Moore Collection, archived at the Louisiana Digital Library

History – Works Progress Administration

WPA projects in the area included dredging and shoreline management as depicted in this diagram by William Spink, the architect working with the Bayou St. John Improvement Assoc.



History – Works Progress Administration

Extensive infrastructure work was accomplished through Works Progress Administration programs of the 1930s, including much of a Bayou St. John improvement plan drafted by Walter Parker. His vision of converting this portion of the bayou from a squalid, neglected, commercial waterway into the Bayou St. John Aquatic Park became a decades-long obsession and was pursued even after his death in 1950 by his widow Anita Hernandez Parker.



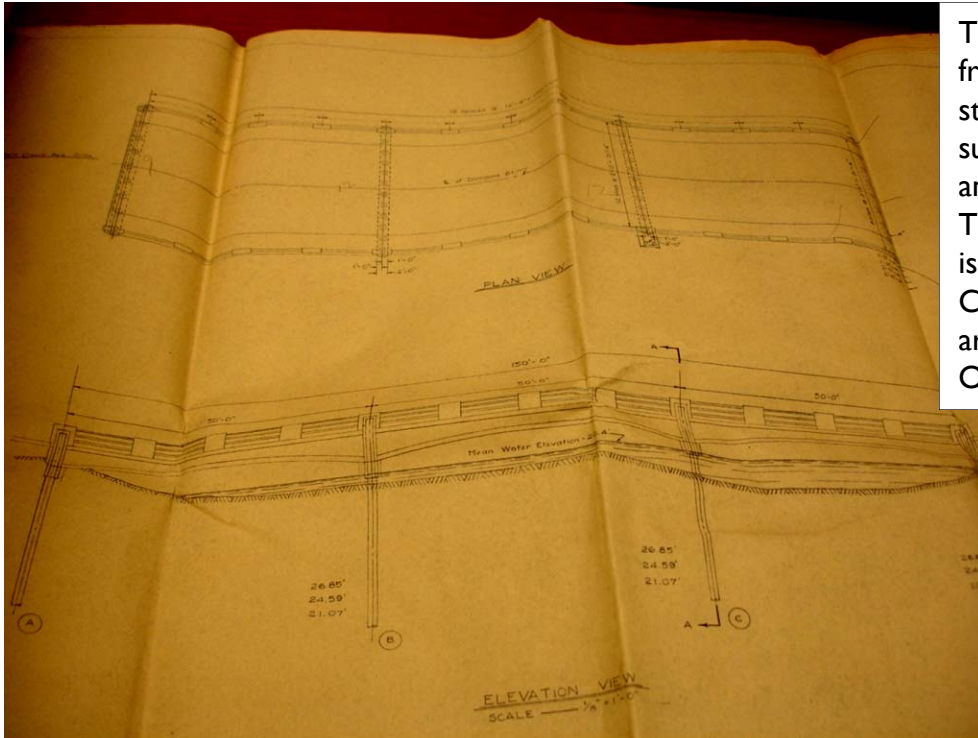
Documents on record at the New Orleans Public Library Archives include a “Descriptive Specification” from 1935 outlining Walter Parker’s eight point plan:

- Revetment: pilings and concrete caps to permanently establish shorelines
- Levees: sloped back from the concrete cap to the roadways
- Roadways: construction of paved roadways (Moss St), curbs and storm drains
- Walks & Boat Landings: paved walkways and boat landings for pleasure boats
- Dredging & Excavating: clearing of trash, stumps and debris from the bayou and the banks, dredging to encourage healthy water flow and grading of the banks
- Concrete Culvert: connecting Jefferson Davis Pkwy with Bayou St. John roadways
- Planting: Bermuda grass and shoreline plantings
- Future Developments:
 - “While this project does not contemplate the full and final development, it provides the foundation, or essential features upon which further and future developments will increasingly make it one of outstanding value, beauty and interest, not only to the citizens of New Orleans, but to their thousands of yearly visitors and sight see-ers.”*

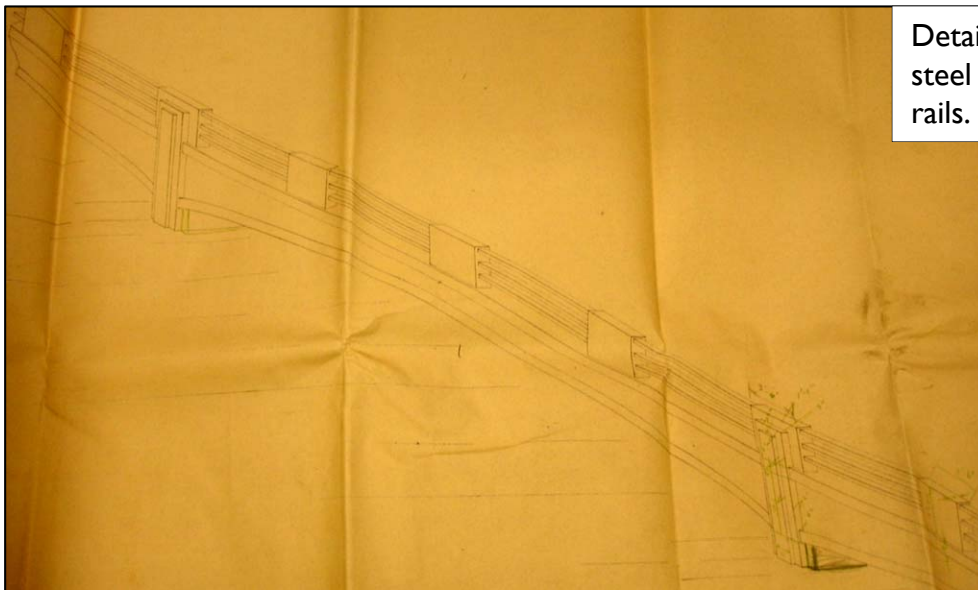
We see this bridge restoration project as one of the ‘further and future developments’ referred to by Mr. Parker some 75 years ago.

History – Dumaine Street Bridge

The Dumaine Street bridge was built in 1951 by the City of New Orleans for \$43,349. It is named for Walter Parker, president of the Bayou St. John Improvement Association from 1927 – 1950, and a tireless advocate for this section of Bayou St. John.



This bridge is a steel framed bridge, with structural concrete supporting the side rails and pedestrian curb. The main roadway deck is steel stringer framed. Complete blueprints are archived at the New Orleans Public Library



Detail of the concrete and steel design of the guard rails.

Bridge plans and blueprints are preserved at the New Orleans Public Library Archives

Current Status

Today the Dumaine Street Bridge has no significant structural damage, but is in need of some cosmetic repair, and a fresh coat of paint.



It was 1961 when structural deterioration was last addressed on the Magnolia Bridge, and fifty years later there is a significant amount of corrosion and disrepair. Fresh paint and some cosmetic repairs occurred in 1989 through the Faubourg St. John Neighborhood Association, but today that paint has faded and peeled.



Complimentary to Other Bayou St. John Area Improvement Projects

Faubourg St. John has accomplished much in its recovery from the floods that followed Hurricane Katrina; several community revitalization and improvement projects in the area, both planned and already completed, will be well complimented by these bridge restorations.

The Wisner Bike Path

The Wisner Bike Path begins where Orleans Avenue crosses Bayou St. John, and the first $\frac{3}{4}$ mile of this 4.7 mile path follows Moss Street into City Park. This \$1.4 million dollar project was completed in October, 2008, greatly increasing bicycle traffic in the area.

The Lafitte Greenway

The 3.1 mile Lafitte Corridor is adjacent to the end of Bayou St. John, and a multi-million dollar project is now underway to convert this idle urban tract into a “greenway that encourages active living, facilitates economic development and links adjacent neighborhoods*...” This long-awaited project will beautify the area and significantly increase biking, walking and watersports along Bayou St. John.



Bayou St. John nutria eradication and repair project

The Coypu Foundation has recently awarded Faubourg St. John Neighborhood Association \$57,000 to study and remediate the Bayou's natural systems, and this project is the subject of academic investigation by the 3rd year graduate studio of LSU's Robert Reich School of Landscape Architecture under the direction of Lake Douglas, PhD, landscape architect and Bayou St. John resident. This investigation, building on previous investigations of the Bayou's natural systems and current water quality data, will result in a strategy for re-vegetating the Bayou in selected locations from the Esplanade Bridge to the Dumaine Bridge, using native plants where appropriate to suggest the character Bayou St. John once had. This strategy builds on the on-going efforts of Louisiana Landmarks to use its site to tell the story of Bayou St. John and its environmental, economic, and social importance to the city of New Orleans.

City Park enhancements facing Bayou St. John

Over 7,000,000 people visited City Park in 2009, and numbers continue to grow as the Park takes on more capital projects. The Big Lake development that faces Bayou St. John at Esplanade Ave has become a popular attraction for walkers, joggers, and paddle-boaters, and more projects are on deck for this area.

* source: “Lafitte Greenway Sustainable Water Design”, August, 2010, Waggonner & Ball Architects

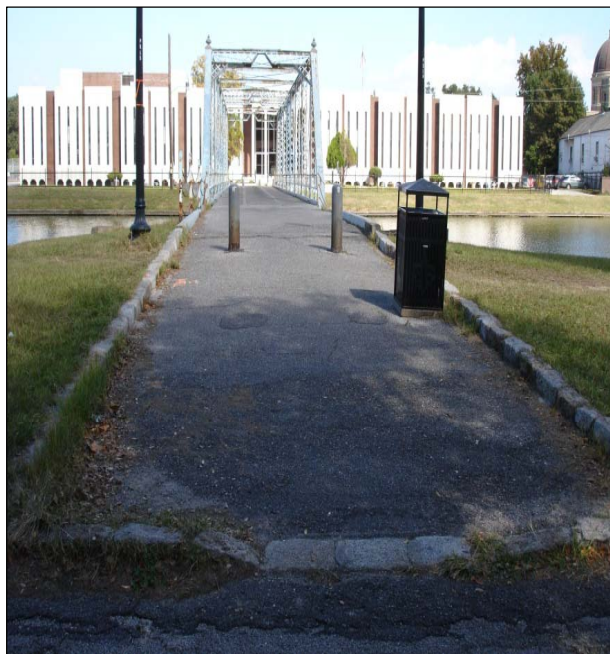
Enhancement Opportunities – Magnolia Bridge



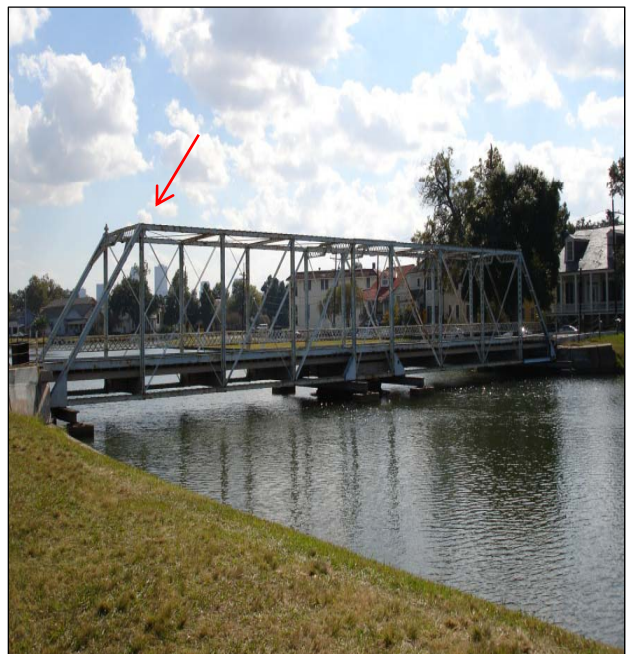
Currently pedestrians and joggers need to step off of the grass area and into the street to walk along the east side of the bayou



Draped chain railing is missing on one side of the west side approach



Asphalt approach is unattractive; could be replaced with cobblestone or brick, and could be a fundraising opportunity



One of the four finials is missing

Support

Wide Community Support for Bayou St. John Bridge Restoration

The Faubourg St. John Neighborhood Association is leading the effort to organize a diverse network of interested parties and stakeholders in the effort to restore and beautify these two historic bridges. Towards that end we feel confident that we can collect letters of support from the following organizations, agencies, and individuals:

Parkview Neighborhood Assoc
MidCity Neighborhood Association
Desaix Neighborhood Assoc
Faubourg St. John Merchants Association
Kristin Gisleson-Palmer, City Councilmember, District C
Susan Guidry, City Councilmember, District A
Arnie Fielkow, At-Large City Councilmember
Jacqueline Clarkson, At-Large City Councilmember
The Pitot House
Cabrini High School
Holy Rosary High School
Holy Rosary Church
Louisiana State University Ag Center
Tulane University School of Architecture
Louisiana Dept of Wildlife & Fisheries
Preservation Resource Center
Louisiana Landmarks Society
Historic Districts Landmark Commission
Louisiana Division of Historic Preservation
Pontchartrain Institute for Environmental Sciences
New Orleans City Park
Lake Pontchartrain Basin Foundation
Friends of Lafitte Corridor
City of New Orleans Department of Parks & Parkways
Orleans Levee Board
Orleans Levee District
Sewerage & Water Board of New Orleans

We propose that representatives from this list of interested parties be invited to participate in a series of facilitated charettes, in order to have the opportunity to provide input and recommendations during the conceptual design phase.

FSJNA is prepared to launch and manage a communications plan to ensure that all stakeholders stay current regarding progress and decisions related to this project. This plan will include neighborhood meetings and email updates monthly and as needed.

APPENDIX

Visual inspections of the Dumaine Street and
Magnolia Bridges were conducted in October, 2010 by
Mr. James R. Danner, Jr, PE

Mr. Danner is Vice President and Principal Engineer at
Denson Engineers, Inc, and is recognized as an expert
in Civil and Structural Engineering.

Magnolia Bridge Refurbishment

City of New Orleans – Capital Improvement Program

Jan. 31, 2018



Agenda

- **Welcome**

James Baker, Mayor's Office of Neighborhood Engagement

- **Project Overview**

Alan Weber, Department of Public Works

- **What to Expect During Construction**

Cheryn Robles, Community Outreach Manager

- **Discussion**

James Baker, Office of Neighborhood Engagement



Ground Rules

- Presentation will precede public comment
- Please limit questions to the project area
- Please fill out a comment card to provide a comment/ask a question
- Please raise your hand and your comment card will be collected by a staff member
- There is a two-minute time limit on questions
- A staff member will hold the microphone for you
- You may not yield time to another speaker



Capital Improvement Program Highlights

- More than **\$2.4B** worth of DPW / SWB work
- Most comprehensive infrastructure program our region has seen in a generation.
- Approximately **200** individual projects - every neighborhood will feel a positive impact.
- ***Multiple Funding Sources:*** FEMA-funded road / waterline work, FEMA-funded Hazard Mitigation Grant Program (HMGP) projects, HUD-funded National Disaster Resiliency Competition (NDRC) grant projects, SWB-funded Sewer System Evaluation and Rehabilitation Program (SSERP) (sewer consent decree) work and City-funded bond project work.
- ***Aggressive project schedules*** coordinated to minimize construction-related impacts.
- ***Work will create thousands of construction jobs*** across the city and maximize economic opportunity for the City's small and disadvantaged businesses.



Magnolia Bridge Rehabilitation

- Funded by the Louisiana Dept. of Transportation and Development, Regional Planning Commission and City bond funds
- Project is valued at approximately **\$1.5M**
- **Engineer:** Volkert, Inc.
- **Construction:** Hard Rock Construction, LLC
- **Schedule:** Construction will begin in February 2018 and last through early 2019



Magnolia Bridge Rehabilitation



Existing Conditions



Rusted metal beams compromise the structural integrity



Existing Conditions



Timber decking is weathered



Scope of Work

- Remove and repair metal grating
- Remove structural paint
- Repair damaged structural elements
- Remove wooden deck and replace with engineered deck
- Install bank stabilization pilings
- Install a cable barrier system
- Re-paint bridge (Federal Blue)
- Minor asphalt approach paving and striping on Moss St.
- Minor sidewalk repairs
- Replace existing bollards



Construction Sequencing

- Remove timber decking and metal beams
- A containment tent will be used to capture sandblasted paint and prevent water and air particle pollution, tent color may vary
- Heavy equipment will be used to install sheet piles (bank stabilization)



Construction Sequencing

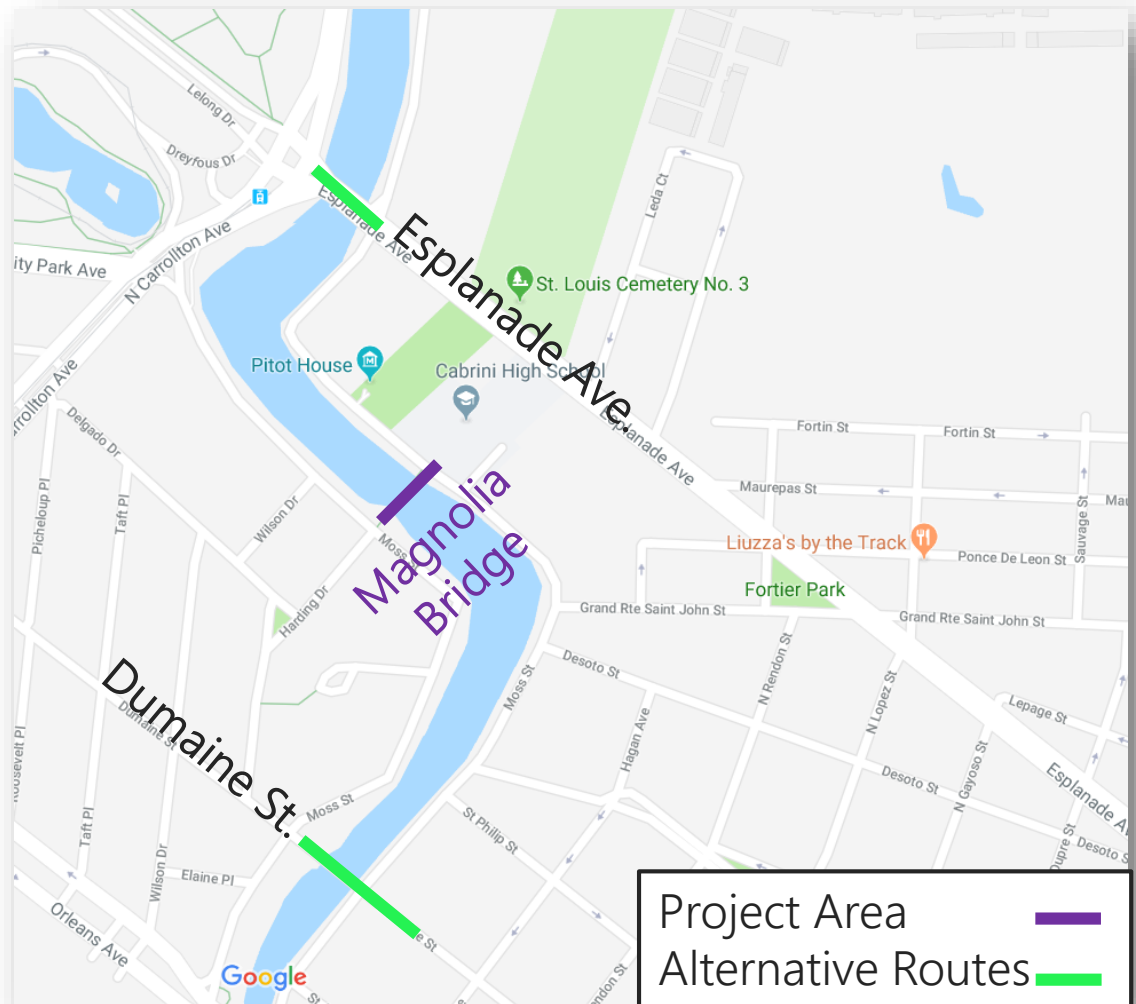
Magnolia Bridge will be unavailable to pedestrians during the rehabilitation project.

Wooden barricades will be placed nightly.

Alternative routes:

- Esplanade Ave.
- Dumaine St.

Crews are authorized to work from 7 a.m. to 5:30 p.m. Monday through Friday. Permission for weekend work may be granted, when necessary.



Schedule

| <u>Milestone</u> | <u>Date</u> |
|-------------------------|---------------|
| Construction Start Date | February 2018 |
| Construction Completed | Early 2019 |

* Please note that all schedules are subject to change and weather delays.



What to Expect During Construction

Temporary construction impacts

- Additional construction equipment
- Sounds
- Dust



Efforts to minimize construction impacts

- Construction signage placed around the sites.
- Any temporary closures will be communicated in advance.
- Any changes to garbage and US Postal services will be communicated in advance.
- Residents will be informed throughout construction.

*Regular work hours are Monday-Friday 7:00 a.m. to 5:30 p.m.
Permission for occasional weekend work may be granted, if required.*



Keeping You Informed

Roadwork.nola.gov

The screenshot shows the Roadwork.nola.gov website interface. At the top, there's a navigation bar with social media icons and the site logo. Below the header is a search bar with the placeholder text "Get Started Here! Enter an address to get block-to-block details on construction projects". A callout points to this bar: "Enter your address here".

On the left side, there's a "Show Layer" section with radio buttons for "Pavement Condition", "Planned Road Construction" (which is selected), and "Roads Under Construction Now". Below this is a "Legend" section listing various construction types with corresponding color-coded lines. A callout points to this section: "Choose to view pavement condition, planned work or roads under construction now".

On the right side, there's a callout pointing to a search icon: "Tell us what you think of the site or download the data".

The main content area displays a map of New Orleans with various construction projects marked. A callout points to a specific project: "Get block-by-block information about the work schedule, type of repairs and what to expect during construction".

Below the map, there's a detailed view of a project on "6800 block of Vicksburg St". It includes a "Pavement Condition" section with a "Failure" link, a "Work Status" section with project details, and a "Schedule" section with start and end dates. A "PLEASE NOTE" section at the bottom states: "The content provided on roadwork.nola.gov is subject to change pending unforeseen site conditions, weather delays, constructability issues, etc."

On the right side of the screenshot, there's a large white box with the text: "CHECK OUT THE NEW FEATURES!" followed by a list of features: "✓mobile-friendly", "✓clean design", "✓improved functionality", and "✓enhanced content". Below this is the website URL "roadwork.nola.gov".



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We have joined NextDoor!



504.658.ROAD (7623)



roadwork@NOLA.gov



Monthly Newsletter Updates

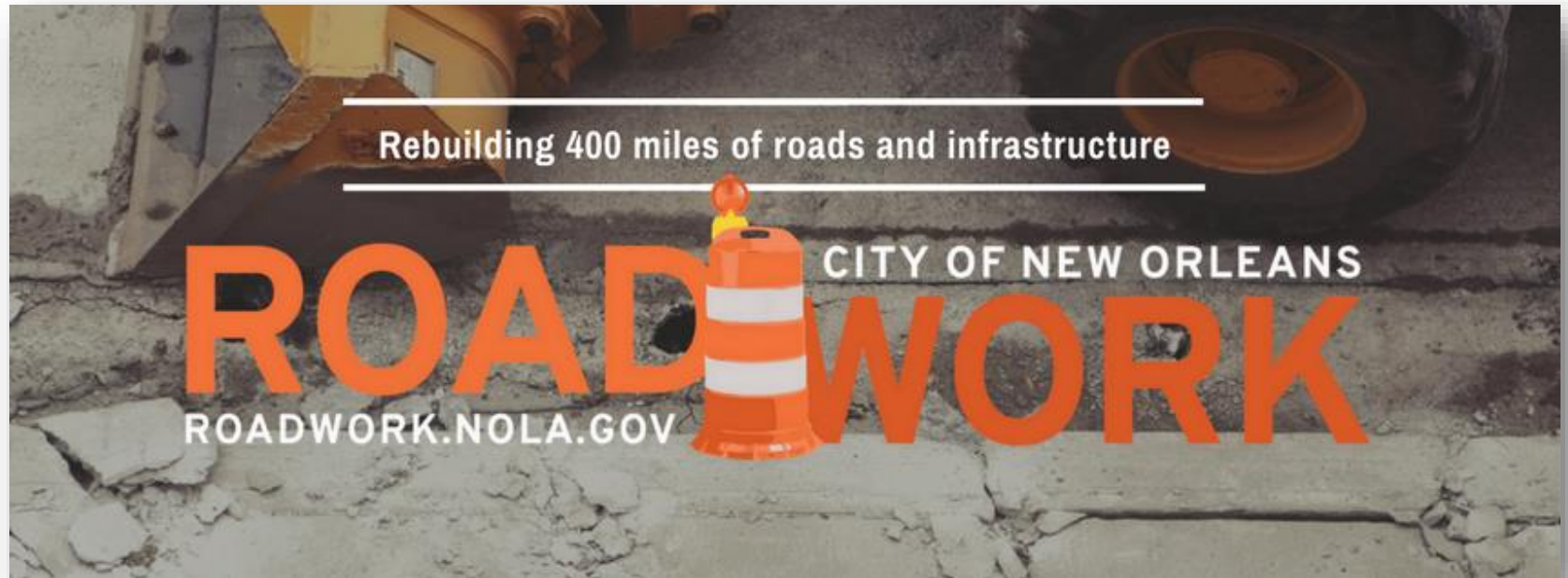


Contact Information

Cheryn Robles, Outreach Manager
504-658-ROAD (7623)
RoadWork@nola.gov



Questions / Discussion



Additional Slides

