

# UPPER (KEESEVILLE) BRIDGE

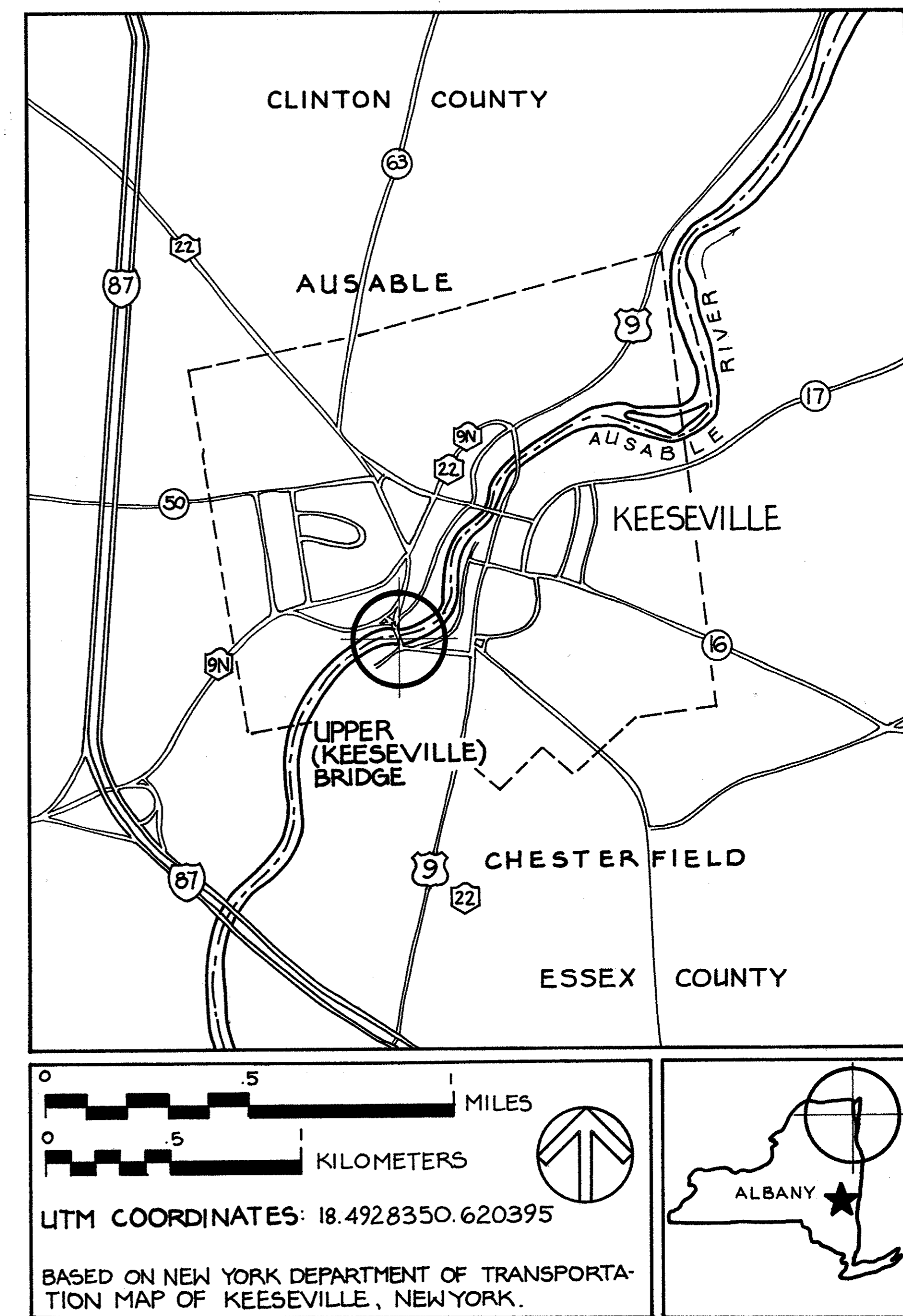
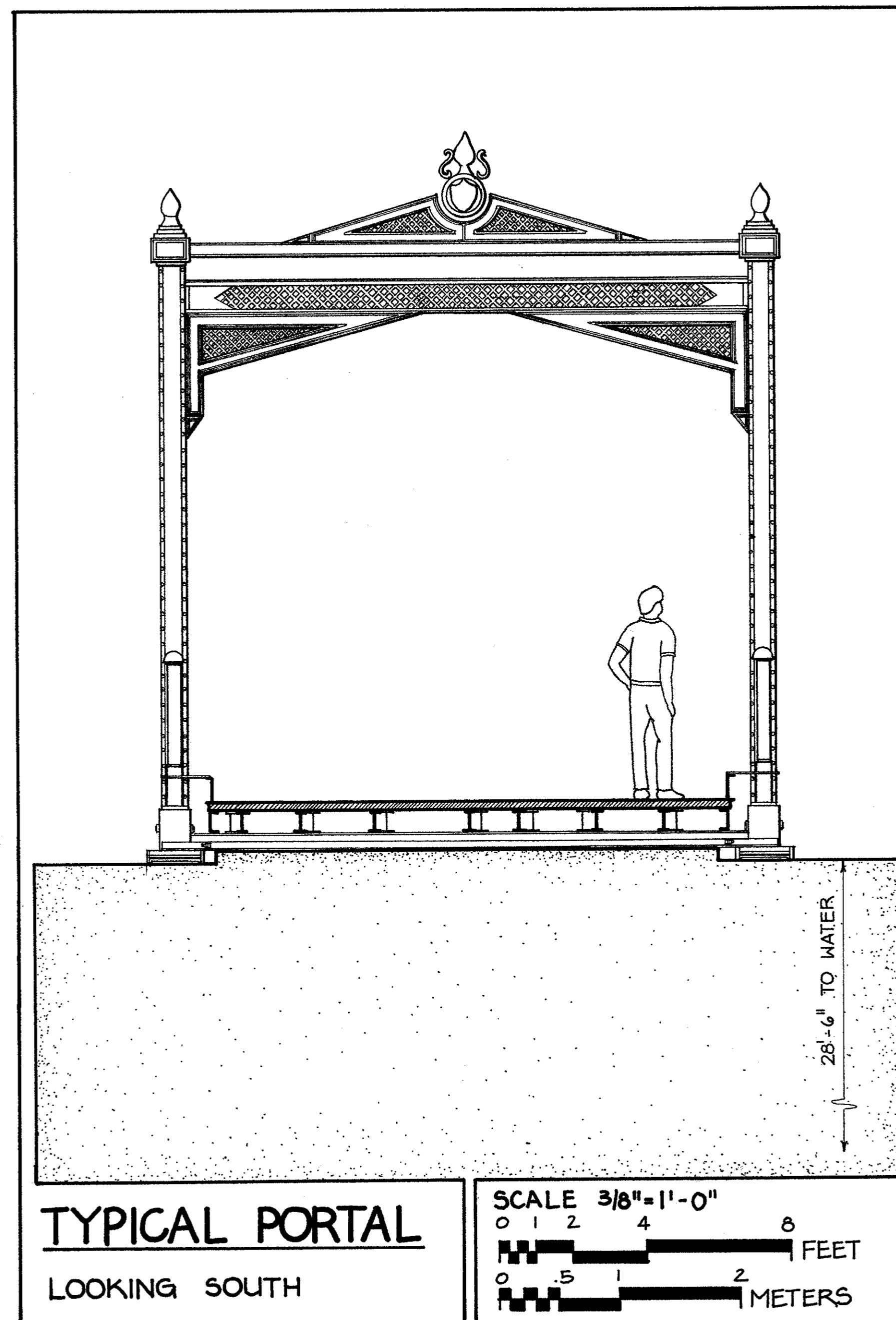
## KEESEVILLE · 1878 · NEW YORK

IN 1878, MURRAY, DOUGAL AND COMPANY OF MILTON, PENNSYLVANIA ERECTED A TWO SPAN, THROUGH PRATT TRUSS BRIDGE ACROSS THE AUSABLE RIVER IN KEESEVILLE, TOWN OF CHESTERFIELD, NEW YORK. THIS BRIDGE REPLACED A COVERED, WOODEN HOWE TRUSS BRIDGE WHICH COLLAPSED DURING 1875. WHEN THE DECISION TO REBUILD THE BRIDGE WAS FINALLY REACHED IN 1877 PROPOSALS WERE RECEIVED FROM A NUMBER OF BRIDGE BUILDERS AND THE CONTRACT WAS AWARDED ON OCTOBER 27, 1877.

MURRAY, DOUGAL AND COMPANY WAS ORIGINALLY ORGANIZED IN 1864 AS A RAILROAD CAR MANUFACTORY BY SAMUEL MURRAY, WILLIAM DOUGAL, JOHN McCLEERY, AND CHARLES McCORMICK, ALL OF MILTON, PENNSYLVANIA. MURRAY, DOUGAL AND COMPANY ENTERED THE BRIDGE BUILDING BUSINESS DURING THE 1870'S AND FABRICATED AND ERECTED BRIDGES UNTIL MAY 1880, WHEN A FIRE DESTROYED BOTH THE BRIDGE AND CAR MANUFACTURING SHOPS. AFTER REBUILDING THE MILTON, PENNSYLVANIA PLANT, THE COMPANY RESUMED MANUFACTURING RAILROAD CARS, BUT WITHDREW FROM BRIDGE BUILDING. IN JANUARY OF 1899, IT BECAME ONE OF THE THIRTEEN COMPANIES AMALGAMATED INTO THE AMERICAN CAR AND FOUNDRY COMPANY.

THIS RECORDING PROJECT IS PART OF THE HISTORIC AMERICAN ENGINEERING RECORD (HAER), A LONG RANGE PROGRAM TO DOCUMENT THE ENGINEERING AND INDUSTRIAL HERITAGE OF THE UNITED STATES. THE HAER PROGRAM IS ADMINISTERED BY THE NATIONAL PARK SERVICE, U.S. DEPARTMENT OF THE INTERIOR. THE NEW YORK HISTORIC BRIDGES RECORDING PROJECT WAS CO-SPONSORED DURING THE SUMMER OF 1987 BY THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION, FRANKLIN E. WHITE, COMMISSIONER; DANIEL J. EGAN, DIRECTOR, PROGRAM PLANNING AND MANAGEMENT GROUP; MARY E. IVEY AND KAREN McCANN, ENVIRONMENTAL ANALYSIS BUREAU; WILLIAM P. CHAMBERLIN, ENGINEERING RESEARCH AND DEVELOPMENT BUREAU.

FIELD WORK, MEASURED DRAWINGS, HISTORICAL REPORTS AND PHOTOGRAPHS WERE PREPARED UNDER THE GENERAL DIRECTION OF DR. ROBERT J. KAPSCH, CHIEF, HISTORIC AMERICAN BUILDINGS SURVEY HISTORIC AMERICAN ENGINEERING RECORD (HAER), AND ERIC DELONY, PRINCIPAL ARCHITECT, HAER. THE RECORDING PROJECT CONSISTED OF CHARLES SCOTT, PROJECT HISTORIAN; HUGH S. O'BRIEN, ARCHITECTURAL SUPERVISOR; ANDREW Q. COLE (MARY WASHINGTON COLLEGE) ASSISTANT HISTORIAN; AND KIM KUYKENDALL (UNIVERSITY OF ARIZONA), ROBIN E. ROSENFELD (TEXAS TECH UNIVERSITY), AND CHARISSA WANG (UNIVERSITY OF MARYLAND), ARCHITECTURAL DELINEATORS. RECORD PHOTOGRAPHS WERE TAKEN BY MARTIN STUPICH. CONSULTING SERVICES WERE PROVIDED BY JOHN R. BOWIE, AIA.



DELINEATED BY: CHARISSA WANG, ROBIN E. ROSENFELD, 1987

NEW YORK BRIDGES RECORDING PROJECT  
HISTORIC AMERICAN ENGINEERING RECORD  
UNITED STATES DEPARTMENT OF THE INTERIOR

UPPER (KEESEVILLE) BRIDGE 1878  
RIVER STREET, OVER THE AUSABLE RIVER, VILLAGE OF KEESEVILLE  
ESSEX COUNTY

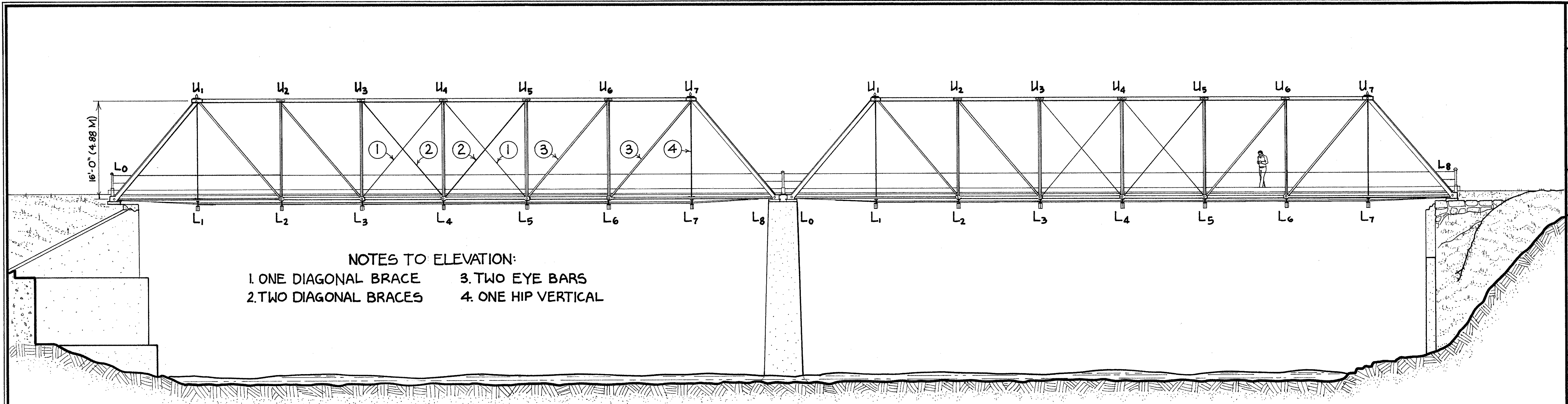
NEW YORK

HISTORIC AMERICAN  
ENGINEERING RECORD  
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SHEET  
1 of 3

UNIVERSITY OF MARYLAND  
1987

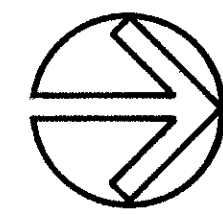
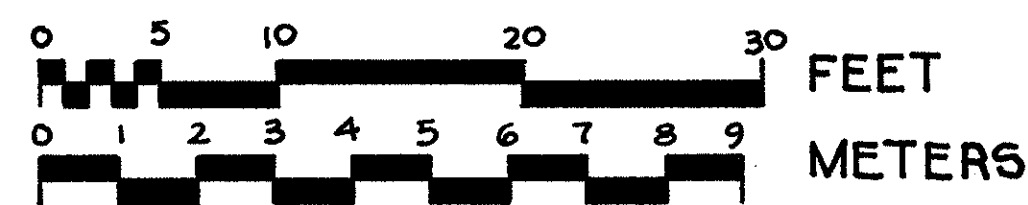
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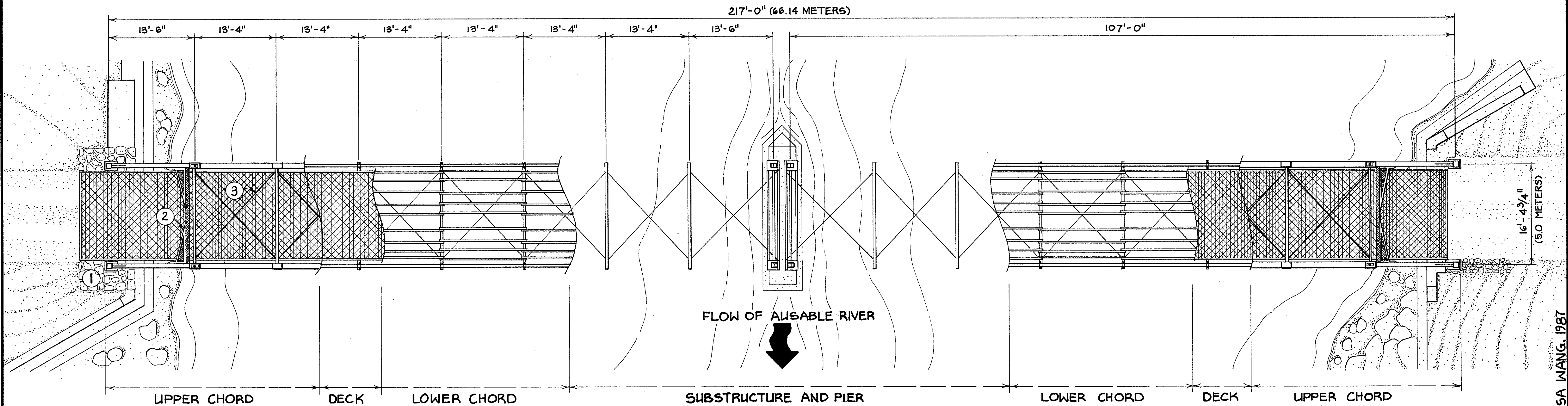
- NOTES TO ELEVATION:
- 1. ONE DIAGONAL BRACE
  - 2. TWO DIAGONAL BRACES
  - 3. TWO EYE BARS
  - 4. ONE HIP VERTICAL

**EAST ELEVATION**

SCALE: 1/8" = 1'-0"



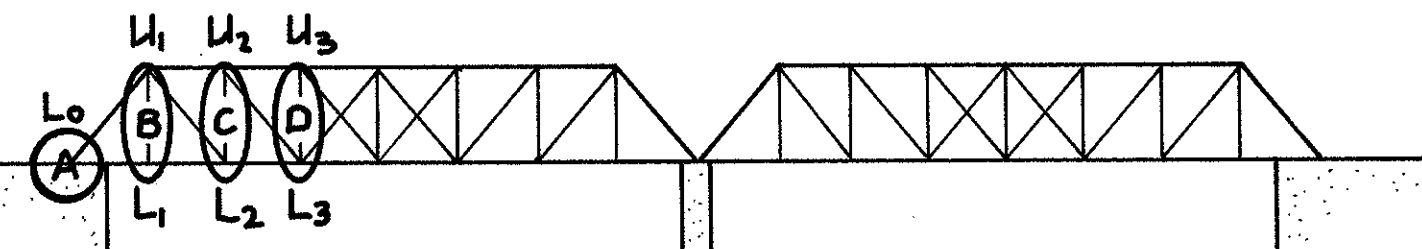
- NOTES TO PLAN:
- 1. ORIGINAL STONE ABUTMENT REINFORCED WITH CONCRETE
  - 2. PORTAL BRACING
  - 3. 3/4" UPPER LATERAL BRACING



**PLAN**

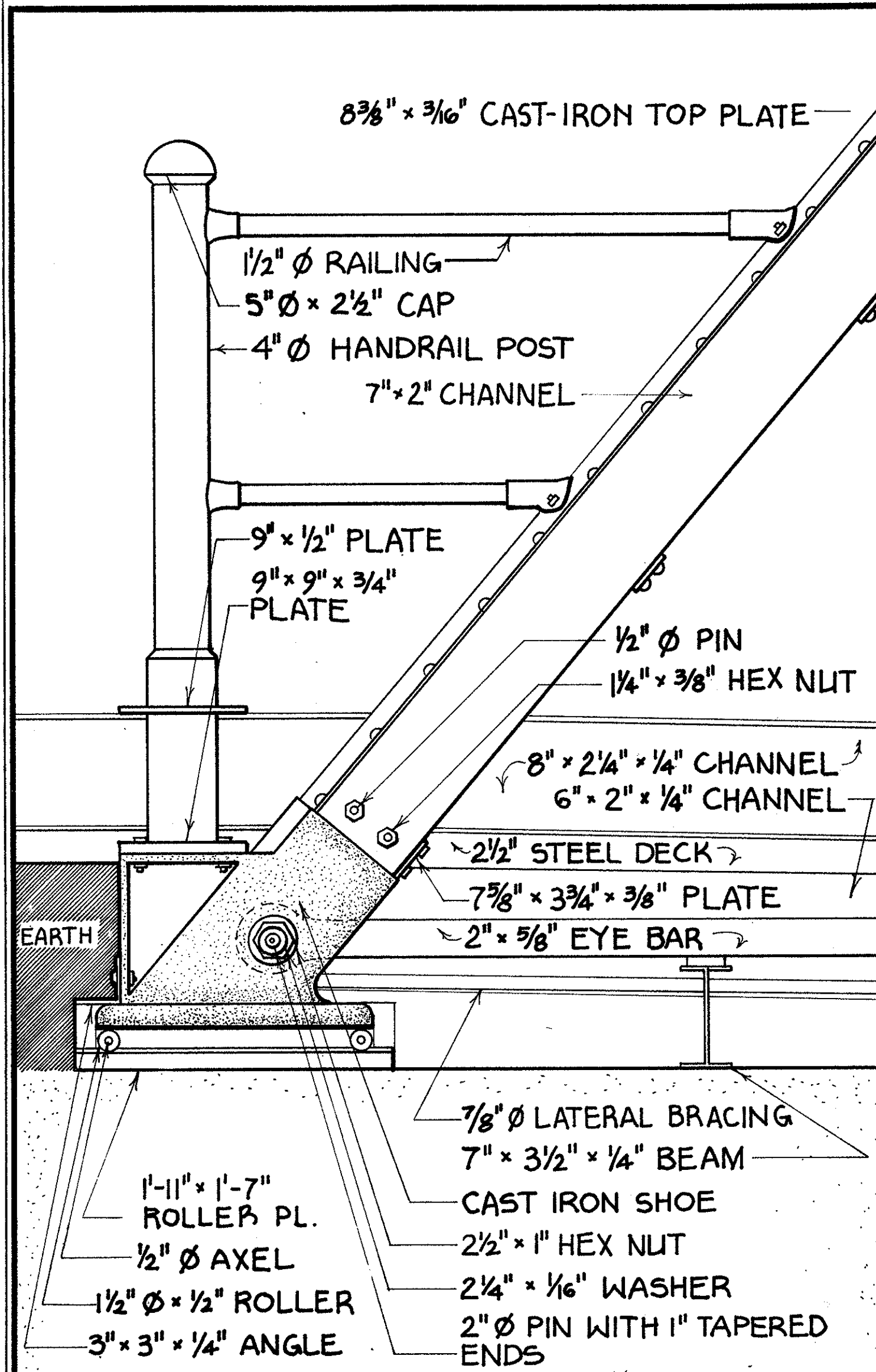
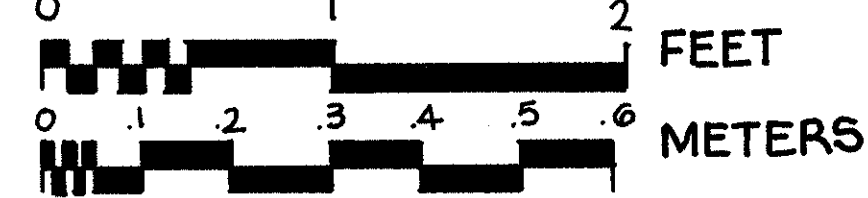
HISTORIC AMERICAN ENGINEERING RECORD  
 SHEET 2 of 3  
 NEW YORK  
 UPPER (KEESEVILLE) BRIDGE, 1878  
 RIVER STREET, OVER THE AUSABLE RIVER, VILLAGE OF KEESEVILLE  
 ESSEX COUNTY  
 KEESEVILLE  
 DELINEATED BY: CHARISSA WANG, 1987  
 NEW YORK BRIDGES RECORDING PROJECT  
 HISTORIC AMERICAN ENGINEERING RECORD  
 UNITED STATES DEPARTMENT OF THE INTERIOR  
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# POST DETAILS

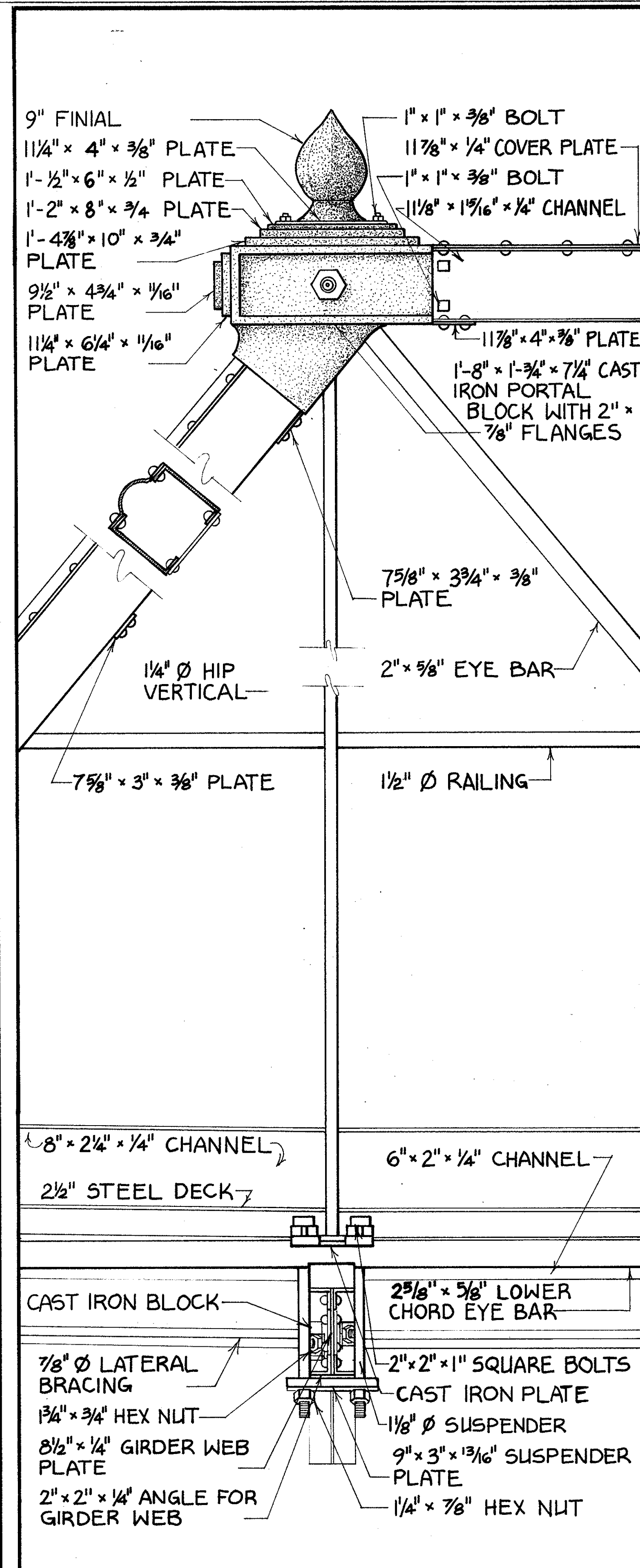


KEY TO EAST ELEVATION NO SCALE

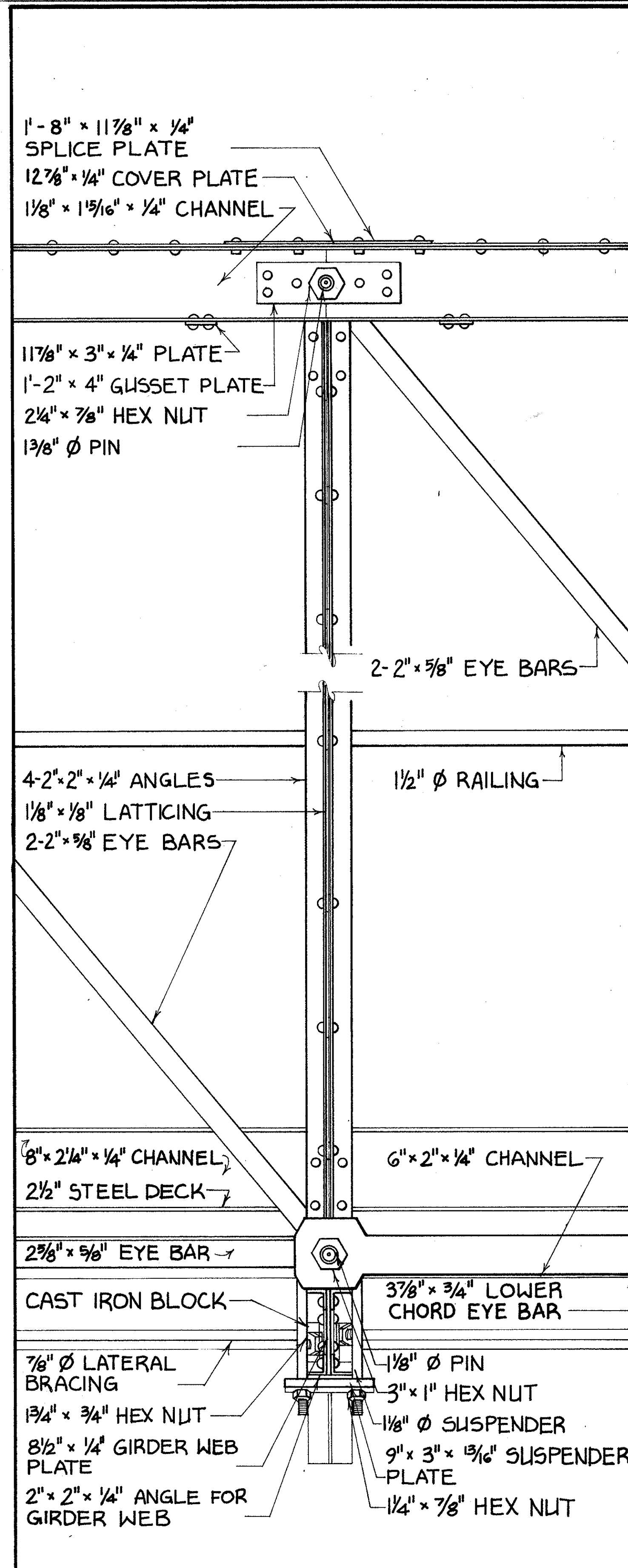
SCALE (DETAILS A, B, C, D): 1/2" = 1'-0"



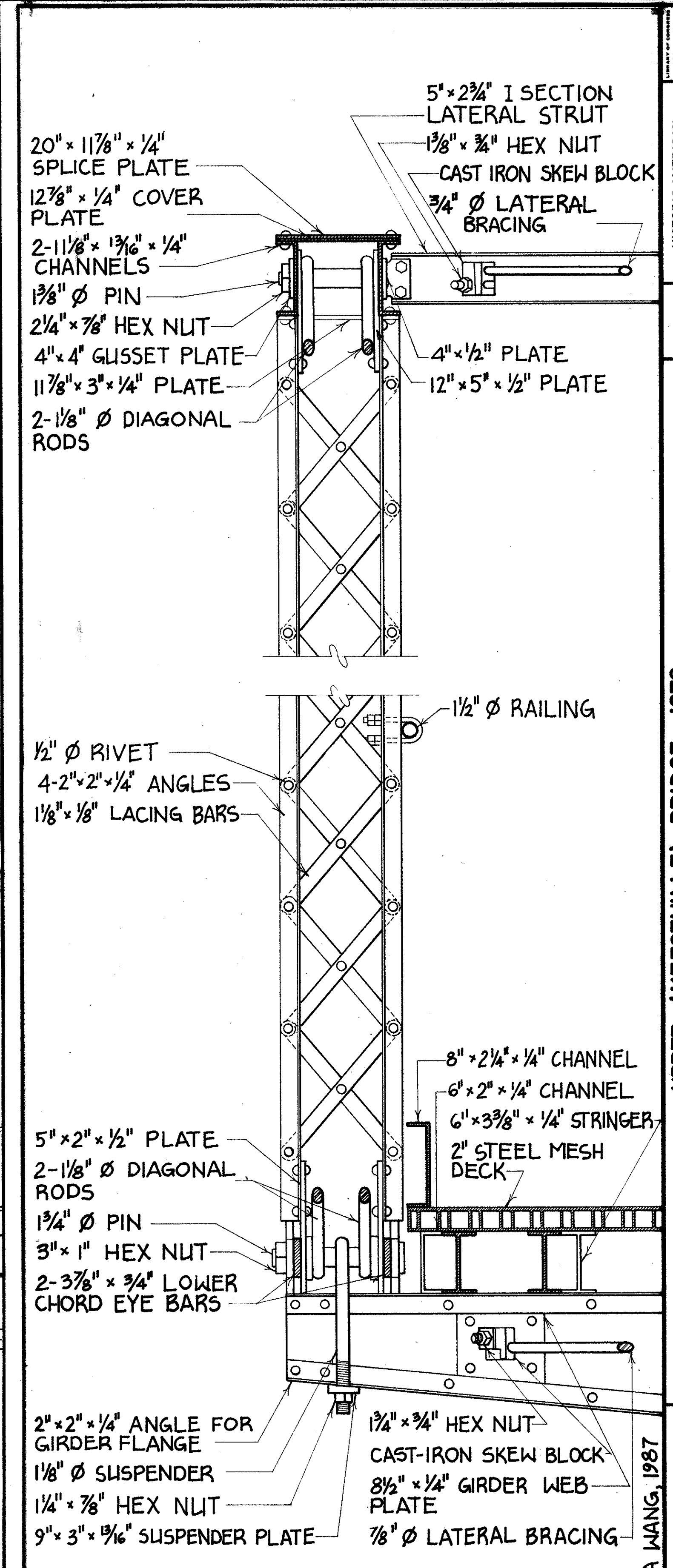
**A) END POST: L<sub>0</sub>**  
ELEVATION LOOKING WEST



**B) POST: U<sub>1</sub> L<sub>1</sub>**  
ELEVATION LOOKING WEST



**C) POST: U<sub>2</sub> L<sub>2</sub>**  
ELEVATION LOOKING WEST



**D) POST: U<sub>3</sub> L<sub>3</sub>**  
SECTION LOOKING NORTH

HISTORIC AMERICAN ENGINEERING RECORD  
 SHEET 3 OF 3  
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