

**ARCHITECTURAL SURVEY REPORT**  
**of**  
**PIN 1757.32**  
**HELDERBERG HUDSON RAIL TRAIL**  
**TOWN OF BETHLEHEM AND CITY OF ALBANY**  
**ALBANY COUNTY, NEW YORK**

**HAA #4711-61**

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*Sponsored by:*  
**US Department of Transportation, FHWA**  
**NYS DOT**  
**Albany County Department of Public Works**

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**April 2014**

## MANAGEMENT SUMMARY

**A. PIN/BIN:** PIN 1757.32

**B. DOT PROJECT TYPE AND FUNDING:** The creation of a pedestrian and bike rail trail on a former track alignment. This project is federally funded with 80% Congestion Mitigation and Air Quality (CMAQ), 20% NFA (Non-Federal Aid) and through a \$500,000 grant obtained from NYS Office of Parks, Recreation and Historic Properties (NYSOPRHP).

**C. CULTURAL RESOURCE SURVEY TYPE:** Architectural Survey.

**D. LOCATION:** The PIN 1757.32 project area is located in the City of Albany and Town of Bethlehem, Albany County, New York (Map 1). The area around the project corridor is a mixture of residential and commercial properties. The existing railroad corridor consists of compacted ballast generally surrounded by trees and natural vegetation. Portions of the corridor are overgrown with vegetation directly adjacent to or covering the existing corridor. The railroad corridor is currently closed to pedestrians; however pedestrian traffic is evident by the existence of a worn path in the vegetation. The existing corridor contains nice views of the Normans Kill. These views are anticipated to be enhanced by the proposed project. The overall visual quality is high.

The project is divided into three phases. The present survey covers the third phase of the project, extending from Delaware Avenue east to South Pearl Street. This phase of the project involves the construction of a 3.18 mile multiuse trail to provide a 10 foot wide asphalt multiuse trail that would accommodate bicycles and pedestrians. This alternative includes the installation of necessary safety items such as bicycle safe fencing, pavement markings and signs, parking lots and other trail amenities. The existing corridor was constructed for rail use and not as a pedestrian trail facility. Consequently, the corridor in its present state contains many safety problems. This concern is elevated in consideration of its intended use as a pedestrian and bicycle trail system and has safety problems such as nonexistent railings, lack of a solid travel surface, minor bridge structural deficiencies.

The project also includes the rehabilitation five bridges (Delaware Avenue, Elsmere Avenue, Sewer Easement, Normans Kill, South Pearl Street) along with seven major culverts and various smaller drainage culverts located throughout. A large concrete tunnel located at the end of Old South Pearl Street near the east end of the project area will also be permanently closed off with a masonry or concrete cap.

The standard trail plus shoulder width is 12-ft. At the Helderberg Hudson Trail over Delaware Avenue, Elsmere Avenue, Bethlehem Sewer Easement, Normans Kill and South Pearl Street bridges the proposed trail plus shoulder width is 10 feet. The minimum allowable trail plus shoulder width is 10 feet therefore the reduction in the overall trail width is not expected to have an impact on the users of the trail. The proposed typical section will be a combination of full depth reconstruction and pavement over graded existing ballast areas. The proposed pavement sections are comprised of 4" of asphalt over 6" of subbase and 4" of asphalt over existing railroad ballast.

This project was submitted and selected for funding under the Transportation Improvement Program. This project is being progressed with both federal and non-federal matching funds from Albany County, who is serving as the Project Sponsor and Applicant. The trail follows an abandoned railroad corridor, previously owned by the Delaware and Hudson Railway Company, between the Town of Voorheesville and the City of Albany. Albany County acquired the property in the year 2009 for the purpose of developing a multiuse recreational facility.

### **E. SURVEY AREA:**

Length: The project area is approximately 3.18 miles (5.1 km) long.

Width: The width of the project area is 12 feet wide (3.7 m), centered on the railroad tracks.

Acreage: The total APE acreage is approximately 9.88 acres (4 ha).

**F. 7.5' QUADRANGLE:** 1993 *Albany and Delmar 7.5' Topographic Quadrangles, New York.*

**G. RESULTS OF ARCHITECTURAL SURVEY:**

Number of structures in project area: 94

Number of known NR listed/eligible structures: 5

Number of recommended eligible structures/districts: 14.

Number of listed/eligible or potentially eligible National Register sites that may be impacted: None.

**H. AUTHOR/CONSULTANT:** Walter Richard Wheeler, Hartgen Archeological Associates, Inc., Rensselaer, New York.

**I. DATE:** April 2014

**J. SPONSOR:** US Department of Transportation, FHWA; NYS DOT; Albany County Department of Public Works

## Table of Contents

DOT PROJECT DESCRIPTION .....	viii
BACKGROUND .....	1
Site File Search.....	1
<i>State and National Registers</i> .....	1
<i>Building Inventories</i> .....	1
ARCHITECTURAL SURVEY .....	2
Introduction .....	2
Survey Methods.....	2
Survey Results.....	3
Historical Synopsis.....	6
<i>The Albany and Susquehanna Railroad</i> .....	6
<i>Adjacent Residential and Commercial Development</i> .....	8
BIBLIOGRAPHY .....	9

### Photograph List

1. Structure 1, [280] Delaware Avenue, constructed 1964, facing north. This structure was determined to be ineligible for listing on the National Register because of significant alterations and additions of recent date.
2. Structure 2, 298 Delaware Avenue, constructed 1910, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
3. Structure 3, BIN 7025300, RR bridge over Delaware Avenue, constructed 1928, facing northeast. This structure has been determined by a previous survey to be eligible for listing on the National Register.
4. Structure 4, 33-35 Herber Avenue, constructed 1951, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
5. Structure 5, 31 Herber Avenue, constructed 1945, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
6. Structure 6, 29 Herber Avenue, constructed 1945, facing north. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
7. Structure 7, 27 Herber Avenue, constructed 1937, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
8. Structure 9, 25 Herber Avenue, constructed 1949, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
9. Structure 10, 23 Herber Avenue, constructed 1937, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
10. Structure 12, 21 Herber Avenue, constructed 1938, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
11. Structure 13, 19 Herber Avenue, constructed 1936, facing north. This structure was determined to be eligible for listing on the National Register under Criterion C.
12. Structure 15, 17 Herber Avenue, constructed 1935, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
13. Structure 17, 13 Herber Avenue, constructed 1937, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
14. Structure 18, 11 Herber Avenue, constructed 1936, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
15. Structure 20, 9 Herber Avenue, constructed 1932, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.



16. Structure 21, 7 Herber Avenue, constructed 1921, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
17. Structure 22, 5 Herber Avenue, constructed 1921, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
18. Structure 24, [5 Elsmere Avenue], constructed c.1910 as a post office, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
19. Structure 25, BIN 7046170, RR bridge over Elsmere Avenue, constructed 1932, facing southeast. This structure has been determined by a previous survey to be eligible for listing on the National Register.
20. Structure 28, 22 West Poplar Drive, constructed 1920, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
21. Structure 29, 24 West Poplar Drive, constructed 1956, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
22. Structure 30, 28 West Poplar Drive, constructed 1955, facing north. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
23. Structure 32, 36 West Poplar Drive, constructed 1960, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
24. Structure 39, Culvert, constructed 1932, facing southeast. This structure was determined to be ineligible for listing on the National Register lack distinguishing characteristics.
25. Structure 42, Blue stone head wall culvert, constructed c.1870, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
26. Structure 57, Rail overpass, by the American bridge Company, constructed c.1890, facing north. This structure has been determined by a previous survey to be eligible for listing on the National Register.
27. Structure 58, Pipe and collapsed stone headwall, constructed c.1890, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
28. Structure 59, Limestone and concrete headwall with iron pipe, constructed c.1890, facing northeast. This structure was determined to be ineligible for listing on the National Register lacks distinguishing characteristics.
29. Structure 61, BIN 2200190, Rockefeller Road bridge, constructed 1914, facing southwest. This structure was determined to be ineligible for listing on the National Register lacks distinguishing characteristics and is in poor condition.
30. Structure 62, Culvert, constructed c.1930, facing northeast. This structure was determined to be ineligible for listing on the National Register lacks distinguishing characteristics.
31. Structure 63, Cattle pass, constructed c.1890, facing northeast. This structure was determined to be eligible for listing on the National Register under Criterion C.
32. Structure 64, Concrete culvert, constructed c.1930, facing northeast. This structure was determined to be ineligible for listing on the National Register lacks distinguishing characteristics.
33. Structure 65, Wood headwall culvert, constructed c.1950, facing north. This structure was determined to be ineligible for listing on the National Register lacks distinguishing characteristics.
34. Structure 66, Stone headwall, constructed c.1868, facing northeast. This structure was determined to be ineligible for listing on the National Register. While this headwall is intact, the north headwall and approximately 20 feet of the brick arch culvert were recently destroyed during hurricane Irene. The structure no longer retains integrity to its period of construction.
35. Structure 67, Culvert with stone walls, constructed c.1890, facing southeast. This structure was determined to be eligible for listing on the National Register under Criterion C.

36. Structure 68, RR Bridge over Normanskill Creek, constructed c.1905, facing southwest. This structure has been determined by a previous survey to be eligible for listing on the National Register.
37. Structure 69, Small bridge with stone endwalls, constructed c.1880, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.
38. Structure 71, BIN 5513549 (southbound), constructed 1954, facing southeast. This structure was determined to be eligible for listing on the National Register.
39. Structure 72, BIN 5512549 (northbound), constructed 1954, facing southwest. This structure was determined to be eligible for listing on the National Register.
40. Structure 76, Ruins of culvert headwall, constructed c.1880, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
41. Structure 78, Former Academy of the Sacred Heart, South Pearl Street, constructed c.1845, c.1875, facing southeast. This structure has been determined by a previous survey to be eligible for listing on the National Register.
42. Structure 79, 1 Boice Street, constructed c. 1845, facing southwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
43. Structure 80, Blue stone head wall culvert, constructed c.1868, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
44. Structure 81, Old South Pearl Street tunnel, constructed 1935, facing southwest. This structure was determined to be ineligible for listing on the National Register lacks distinguishing characteristics.
45. Structure 83, 812 Old South Pearl Street, constructed c.1910, facing east. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
46. Structure 84, 810 Old South Pearl Street, constructed c.1900, facing southeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
47. Structure 85, 798 Old South Pearl Street, constructed c.1890, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
48. Structure 86, 782 Old South Pearl Street, constructed c.1840, facing southeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
49. Structure 87, 778 Old South Pearl Street, constructed c.1875, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.
50. Structure 91, BIN 7022460, RR Bridge over South Pearl Street, constructed 1932, facing north. This structure was determined to be eligible for listing on the National Register under Criterion C.
51. View looking northeast at 23 Herber Avenue.
52. View looking north-northwest at 23 Herber Avenue.
53. View of 21 Herber Avenue, looking north-northwest.
54. View of 21 Herber Avenue, looking northeast.
55. View looking northwest at 19 Herber Avenue.
56. View looking north-northeast at 19 Herber Avenue.
57. View looking northwest at 17 Herber Avenue.
58. View looking northeast at 17 Herber Avenue.
59. View looking southwest at 17 Herber Avenue.
60. View looking northwest at 13 Herber Avenue.
61. View looking east-northeast at 13 Herber Avenue.

62. View looking north-northwest at 11 Herber Avenue.
63. View looking northeast at 11 Herber Avenue.
64. View looking south at 11 Herber Avenue and its associated garage.
65. View looking northeast at 9 Herber Avenue.
66. View looking northwest, showing relationship between house and garage.
67. View northwest showing garage.
68. View southwest.
69. View looking north-northwest at 22 West Poplar Drive.
70. View looking north-northeast at 22 West Poplar Drive.
71. View looking south at 22 West Poplar Drive, with detached garage at center left.
72. View looking northeast at the cattle pass.
73. View northeast, showing typical timber and stone abutment detail.
74. View southeast, showing underside of wood deck and abutment detail.
75. View looking southwest at culvert/cattle pass.
76. View northwest toward culvert/cattle pass.
77. View north showing details of abutments and steel rail-supported deck.
78. Detail showing section of rails used for decking.
79. General view looking northwest.
80. View looking southwest.
81. View east-southeast showing the main span.
82. View south showing the abutment and southern span (at right).
83. View looking north-northwest, showing north abutment, at left.
84. View west looking at bridge.
85. View southwest, showing south abutment at left center. Contemporary southbound bridge is at right.
86. View west-northwest, showing north abutment.
87. View north from South Pearl Street.
88. View west at deck level.
89. View northeast showing east abutment and underside of deck.
90. Bridge plaque, mounted to the east end of the north plate girder.

**Table List**

1. National Register (NR) and National Register Eligible (NRE) Properties within 0.5 miles (0.8 km) of the PIN 1757.32 Project Area .....	1
2. Structures Previously Inventoried within 0.5 miles (0.8 km) of the PIN 1757.32 Project Area.....	1-2
3. Structures within/adjacent to the PIN 1757.32 Project Area. ....	3-5
4. Potential Landscape Impacts to NRE* properties within/adjacent to the PIN 1757.32 Project Area.....	6

**Map List**

1. Project Location (NYSDOT 1993a, 1993b).	
2-13. Project Maps (Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)	

## DOT PROJECT DESCRIPTION

An architectural survey of PIN 1757.32 project area, in the Town of Bethlehem and City of Albany, both in Albany County, New York, was completed by Hartgen Archeological Associates, Inc. (HAA, Inc.) in April 2014. The field reconnaissance was conducted according to guidelines and specifications contained within the *New York State Education Department Cultural Reconnaissance Survey Program Work Scope Specifications for Cultural Resource Investigations* (NYSED 2004). The survey included identification of NRE and listed structures within the APE and completion of Building/Structure Inventory Forms for potentially NRE structures which have not previously been surveyed. The project area boundaries were determined by information provided by Greenman-Pedersen, Inc.

The area around the project corridor is a mixture of residential and commercial properties. The existing railroad corridor consists of compacted ballast generally surrounded by trees and natural vegetation. Portions of the corridor are overgrown with vegetation directly adjacent to or covering the existing corridor. The railroad corridor is currently closed to pedestrians; however pedestrian traffic is evident by the existence of a worn path in the vegetation. The existing corridor contains nice views of the Normans Kill. These views are anticipated to be enhanced by the proposed project. The overall visual quality is high.

The project is divided into three phases. The present survey covers the third phase of the project, extending from Delaware Avenue east to South Pearl Street. This phase of the project involves the construction of a 3.18 mile multiuse trail to provide a 10 foot wide asphalt multiuse trail that would accommodate bicycles and pedestrians. This alternative includes the installation of necessary safety items such as bicycle safe fencing, pavement markings and signs, parking lots and other trail amenities. The existing corridor was constructed for rail use and not as a pedestrian trail facility. Consequently, the corridor in its present state contains many safety problems. This concern is elevated in consideration of its intended use as a pedestrian and bicycle trail system and has safety problems such as nonexistent railings, lack of a solid travel surface, minor bridge structural deficiencies.

The project also includes the rehabilitation five bridges (Delaware Avenue, Elsmere Avenue, Sewer Easement, Normans Kill, South Pearl Street) along with seven major culverts and various smaller drainage culverts located throughout. A large concrete tunnel located at the end of Old South Pearl Street near the east end of the project area will also be permanently closed off with a masonry or concrete cap.

The standard trail plus shoulder width is 12-ft. At the Helderberg Hudson Trail over Delaware Avenue, Elsmere Avenue, Bethlehem Sewer Easement, Normans Kill and South Pearl Street bridges the proposed trail plus shoulder width is 10 feet. The minimum allowable trail plus shoulder width is 10 feet therefore the reduction in the overall trail width is not expected to have an impact on the users of the trail. The proposed typical section will be a combination of full depth reconstruction and pavement over graded existing ballast areas. The proposed pavement sections are comprised of 4" of asphalt over 6" of subbase and 4" of asphalt over existing railroad ballast.

This project was submitted and selected for funding under the Transportation Improvement Program. This project is being progressed with both federal and non-federal matching funds from Albany County, who is serving as the Project Sponsor and Applicant. The trail follows an abandoned railroad corridor, previously owned by the Delaware and Hudson Railway Company, between the Town of Voorheesville and the City of Albany. Albany County acquired the property in the year 2009 for the purpose of developing a multiuse recreational facility.

## BACKGROUND

### Site File Search

#### *State and National Registers*

A review of the OPRHP inventory in April 2014 discovered no previously listed resources located within .5 miles (0.8 km) of the project area.

**Table 1. National Register (NR) and National Register Eligible (NRE) Properties within 0.5 miles (0.8 km) of the PIN 1757.32 Project Area.**

Site Name/Number	Site Description	Site Location	Status
None identified	n/a	n/a	n/a

#### *Building Inventories*

Bridge BIN 3313880 has previously been determined to be not eligible for listing on the National Register by the Historic Bridge Inventory Project conducted in 2002. No other previously surveyed or inventoried structures were identified by the site file search (Table 2).

**Table 2. Structures Previously Inventoried within 0.5 miles (0.8 km) of the PIN 1757.32 Project Area.**

Site Name/Number	Structure # This Survey	Site Description	Site Location	Status
7 Rockefeller Road / 00102.000144	--		Town of Bethlehem	Not NRE
11 Rockefeller Road / 00102.000145	--	Building E2 (DOT PIN #1090.04)	Town of Bethlehem	Not NRE
00102.000146	--	Building F2 (DOT PIN #1090.04)	West side; south of Delaware Avenue, Town of Bethlehem	Not NRE
24 Rockefeller Road / 00102.000147	--	Building G2 (DOT PIN #1090.04)	Town of Bethlehem	Not NRE
14 Rockefeller Road / 00102.000148	--	Building H2 (DOT PIN #1090.04)	Town of Bethlehem	Not NRE
12 Rockefeller Road / 00102.000149	--	Building I2 (DOT PIN #1090.04)	Town of Bethlehem	Not NRE
10 Rockefeller Road / 00102.000150	--	Building J2 (DOT PIN #1090.04)	Town of Bethlehem	NRE
Good Samaritan Nursing Home / 00102.000210	60		Town of Bethlehem	No determination made
D&H Railroad Bridge over Normanskill / 00102.000581	68	bridge which currently connects Town of Bethlehem and City of Albany	Town of Bethlehem	NRE
D&H Railroad Trestle / 00102.000582 and	57	Trestle over Bethlehem Sewer Easement; "Mason	Mason Road, Town of Bethlehem	NRE as part of a District

Site Name/Number	Structure # This Survey	Site Description	Site Location	Status
00140.004774		Road Bridge.” Stone abutments, steel plate girder type		
St. Stephens Episcopal / 00102.000645	--	Episcopal Church	Elsmere Avenue at Poplar Drive, Town of Bethlehem	No determination Made
Bethlehem Center School / 00102.000746	--	Bethlehem Center School	397 US Route 9W, Town of Bethlehem	NRE
CP Rail Bridge over Route 443 / 00102.000785	3	1932 steel plate girder type	Delaware Avenue / State Route 443, Town of Bethlehem	NRE as part of a district
CP Rail Bridge over Elsmere Avenue / 00102.000887	25	1932 steel plate girder type	Elsmere Avenue (NY Route 335), Town of Bethlehem	NRE as part of a district
Kenwood Estate / 00140.004638	78	Academy of the Sacred Heart (formerly Kenwood Estate)	Between NY 32 and 9W, south of 787, City of Albany	NRE as part of a district
126 Port South Road / 00140.004771	94	Harris Rebar / 20 <sup>th</sup> c Aluminum industrial building	Port of Albany, City of Albany	Not NRE

## ARCHITECTURAL SURVEY

### Introduction

The architectural reconnaissance survey was performed by HAA, Inc.’s staff on April 1, 2014. The survey was performed according to the *NYS Education Department Cultural Resources Survey Program Work Scope Specifications for Cultural Resource Investigations* (NYSED 2004). The goal of the survey was to identify properties that are potentially eligible for listing on the National Register of Historic Places.

The PIN 1757.32 project area is located in the City of Albany and Town of Bethlehem, Albany County, New York. The project is divided into three phases. The present survey covers the third phase of the project, extending from Delaware Avenue east to South Pearl Street. This phase of the project involves the construction of a 3.18 mile multiuse trail to provide a 10 foot wide asphalt multiuse trail that would accommodate bicycles and pedestrians. This alternative includes the installation of necessary safety items such as bicycle safe fencing, pavement markings and signs, parking lots and other trail amenities. The project also includes the rehabilitation five bridges (Delaware Avenue, Elsmere Avenue, Sewer Easement, Normans Kill, South Pearl Street) along with seven major culverts and various smaller drainage culverts located throughout. A large concrete tunnel located at the end of Old South Pearl Street near the east end of the project area will also be permanently closed off with a masonry or concrete cap.

### Survey Methods

The architectural survey consisted of the following: (1) a review of the OPRHP structure files to identify National Register (NR) listed or eligible properties and previously inventoried properties within the project area, and (2) a field inspection of structures located within and adjacent to the project area. Each property was identified by street number and its location was recorded on the project map (Fig. 2). All pre-1965 structures were evaluated for their potential ability to satisfy one or more of the four National Register criteria for significance: (A) by association with events that have made a significant contribution to the broad patterns of history; (B) by association with the

lives of persons significant in the past; (C) by embodying the distinctive characteristics of a type, period, or method of construction; or (D) by potentially yielding information important to history (Shrimpton 1997).

### Survey Results

The architectural survey included 94 individual structures. The list of Architectural Properties (Table 3) associates these structures with structure and photograph numbers. These are keyed to maps (Maps 2 thru 13). No structures are listed on the National Register. Five structures (Structures 3, 25, 57, 68 and 78) were identified by previous surveys to be eligible for individual listing on the State and National Registers, or eligible as part of a district (Photos 3, 19, 26, 36, and 41). Fourteen structures (Structures 10, 12, 13, 15, 17, 18, 20, 28, 63, 67, 69, 71, 72 and 91) have been identified by this survey as potentially eligible for listing on the State and National Registers (Photos 9, 10, 11, 12, 13, 14, 15, 20, 31, 35, 37, 38, 39 and 50). An additional 31 structures were identified as being in excess of 50 years in age but not eligible for listing on either register chiefly due to low levels of integrity.

Photographs are provided for all 50 individual or associated structures which are located within or adjacent to the project area and which are in excess of 50 years old (Photos 1 through 50). Building Structure Inventory forms and NYS DOT Bridge Inventory forms have been completed for NRE resources identified by this survey and are appended to this report in Appendix I. Project impacts to eligible properties are limited to the installation of safety and pedestrian fencing and replacement of decking to bridges and overpasses. There are no known landscape impacts to eligible structures (Table 4).

**Table 3. Structures within/adjacent to the PIN 1757.32 Project Area.**

Structure	Photo	Map	Name/Address	NRE*	Non-NRE	Post 1964	SHPO Unique Site #/ Existing NR Status
1	1	2	[280] Delaware Avenue		y		
2	2	2	298 Delaware Avenue		y		
3	3	2	BIN 7025300, RR bridge over Delaware Avenue	x			102.000785, NRE
4	4	2	33-35 Herber Avenue		y		
5	5	2	31 Herber Avenue		y		
6	6	2	29 Herber Avenue		y		
7	7	2	27 Herber Avenue		y		
8	-----	2	282 Delaware Avenue		y	x	
9	8	2	25 Herber Avenue		y		
10	9	2	23 Herber Avenue	x			
11	-----	2	276 Delaware Avenue		y	x	
12	10	2	21 Herber Avenue	x			
13	11	2	19 Herber Avenue	x			
14	-----	2	274 Delaware Avenue		y	x	
15	12	2	17 Herber Avenue	x			
16	-----	2	270 Delaware Avenue		y	x	
17	13	2	13 Herber Avenue	x			
18	14	2	11 Herber Avenue	x			
19	-----	2	266 Delaware Avenue		y	x	
20	15	2	9 Herber Avenue	x			
21	16	2	7 Herber Avenue		y		
22	17	2	5 Herber Avenue		y		



Structure	Photo	Map	Name/Address	NRE*	Non-NRE	Post 1964	SHPO Unique Site #/ Existing NR Status
23	-----	3	260 Delaware Avenue		y	x	
24	18	3	[5 Elsmere Avenue]		y		
25	19	3	BIN 7046170, RR bridge over Elsmere Avenue	x			102.000887, NRE
26	-----	3	250 Delaware Avenue		y	x	
27	-----	3	16 West Poplar Drive		y	x	
28	20	3	22 West Poplar Drive	x			
29	21	3	24 West Poplar Drive		y		
30	22	3	28 West Poplar Drive		y		
31	-----	3	32 West Poplar Drive		y	x	
32	23	3	36 West Poplar Drive		y		
33	-----	3	40 West Poplar Drive		y	x	
34	-----	3	44 West Poplar Drive		y	x	
35	-----	3	48 West Poplar Drive		y	x	
36	-----	3	50 West Poplar Drive		y	x	
37	-----	4	72 East Poplar Drive		y	x	
38	-----	4	74 East Poplar Drive		y	x	
39	24	4	Culvert		y		
40	-----	4	76 East Poplar Drive		y	x	
41	-----	4	78 East Poplar Drive		y	x	
42	25	4	Blue stone head wall culvert		y		
43	-----	4	88 East Poplar Drive		y	x	
44	-----	4	92 A&B East Poplar Drive		y	x	
45	-----	4	96 East Poplar Drive		y	x	
46	-----	4	Metal pipe culvert		y	x	
47	-----	4	100 A&B East Poplar Drive		y	x	
48	-----	4	104 A&B East Poplar Drive		y	x	
49	-----	4	108 A&B East Poplar Drive		y	x	
50	-----	4	110 A&B East Poplar Drive		y	x	
51	-----	4	114 A&B East Poplar Drive		y	x	
52	-----	5	118 East Poplar Drive		y	x	
53	-----	5	120 East Poplar Drive		y	x	
54	-----	5	124 East Poplar Drive		y	x	
55	-----	5	126 East Poplar Drive		y	x	
56	-----	5	130 East Poplar Drive		y	x	
57	26	5	Rail overpass, by the American bridge Company	x			102.000582; 00140.004774, NRE
58	27	5	Pipe and collapsed stone headwall		y		
59	28	6	Limestone and concrete headwall with iron pipe		y		
60	-----	6	119-125 Rockefeller Road		y	x	

Structure	Photo	Map	Name/Address	NRE*	Non-NRE	Post 1964	SHPO Unique Site #/ Existing NR Status
61	29	6	BIN 2200190, Rockefeller Road bridge		y		
62	30	6	Culvert		y		
63	31	7	Cattle pass	x			
64	32	7	Concrete culvert		y		
65	33	8	Wood headwall culvert		y		
66	34	8	Stone headwall		y		
67	35	8	Culvert with stone walls	x			
68	36	9	RR Bridge over Normanskill Creek	x			102.000581, NRE
69	37	9	Small bridge with stone endwalls	x			
70	-----	10	Galvanized culvert pipe		y	x	
71	38	10	BIN 5513549 (southbound)	x			
72	39	10	BIN 5512549 (northbound)	x			
73	-----	10	Culvert		y	x	
74	-----	10	Concrete culvert		y	x	
75	-----	10	BIN 1007610, Rt 9W bridge		y	x	
76	40	11	Ruins of culvert headwall		y		
77	-----	11	Concrete headwall		y	x	
78	41	12	Former Academy of the Sacred Heart, South Pearl Street	x			140.004638, NRE
79	42	11	1 Boice Street		y		
80	43	11	Blue stone head wall culvert		y		
81	44	11	Old South Pearl Street tunnel		y		
82	-----	11	7 Binghamton Street		y	x	
83	45	11	812 Old South Pearl Street		y		
84	46	11	810 Old South Pearl Street		y		
85	47	11	798 Old South Pearl Street		y		
86	48	13	782 Old South Pearl Street		y		
87	49	13	778 Old South Pearl Street		y		
88	-----	13	75 Old South Pearl Street		y	x	
89	-----	13	76 Old South Pearl Street		y	x	
90	-----	13	77 Old South Pearl Street		y	x	
91	50	13	BIN 7022460, RR Bridge over South Pearl Street	x			
92	-----	13	850 South Pearl Street		y	x	
93	-----	13	722 South Pearl Street		y	x	
94	-----	13	126 Port South Road		y	x	140.004771, determined ineligible

\* NRE = National Register Eligible

**Table 4. Potential Landscape Impacts to NRE\* properties within/adjacent to the PIN 1757.32 Project Area.**

Structure	Property Name	SHPO Unique Site #/Existing NR Status	Other Inventory	Anticipated Landscape Impacts
3	BIN 7025300, RR bridge over Delaware Avenue	102.000785		none
10	23 Herber Avenue			none
12	21 Herber Avenue			none
13	19 Herber Avenue			none
15	17 Herber Avenue			none
17	13 Herber Avenue			none
18	11 Herber Avenue			none
20	9 Herber Avenue			none
25	BIN 7046170, RR bridge over Elsmere Avenue	102.000887		none
28	22 West Poplar Drive			none
57	Rail overpass, built by the American bridge Company	102.000582; 00140.004774		none
63	Cattle pass			none
67	Culvert with stone walls			none
68	RR Bridge over Normanskill Creek	102.000581		none
69	Small bridge with stone endwalls			none
78	Former Academy of the Sacred Heart, South Pearl Street	140.004638		none
71	BIN 5513549 (southbound)			none
72	BIN 5512549 (northbound)			none
91	BIN 7022460, RR Bridge over South Pearl Street			none

\* NRE = National Register Eligible

## Historical Synopsis

### *The Albany and Susquehanna Railroad*

The path taken by the former Albany and Susquehanna Railroad (later Delaware and Hudson, and finally CSX) between what was, in the nineteenth century, Kenwood at the east and present-day Elsmere, at the west, was first established as a rail line in the early 1860s. The course of the east end of the line was selected to pass through the village of Kenwood, then south of the corporate bounds of the City of Albany, and seat of several manufacturing concerns (Albany Evening Journal 1855:2). A connecting rail line for passenger traffic was contemporaneously developed to link Kenwood with downtown Albany. The Albany and Kenwood Railroad was established in 1863 and track for a horse-drawn railroad was laid in South Pearl Street in 1865-66 (Dunn 1906:502; Daily Albany Argus 1865:1). The line opened in 1866 (Albany Evening Journal 1866:3).

The Albany and Susquehanna Railroad was established in 1851 (Dunn 1906:502). The Chief Engineer for the railroad was Charles W. Wentz. Work was completed at the Albany end of the line first; the railroad was fully operational in January 1869, and initially provided both freight and passenger service (Dana 1903:214, 219). The stone headwall of a culvert (Structure 66; Photo 34) probably represents work typical of this first period of the railroad's history. After a significant controversy involving sabotage and an attempt at a hostile takeover by Jay

Gould and his associates, the tracks were leased to the Delaware & Hudson Railroad in 1870 in order to circumvent further problems (Dana 1903:220-228; Colby 1882:608).

Crossings over the Normanskill portion of the rail line have frequently been subject to washouts, particularly in the nineteenth century. Floods removed all the crossings of the stream in 1856, previous to the establishment of the line (Albany Evening Journal 1856:2). The original (c.1863) railroad bridge over the Normanskill was taken out by a flood in October 1869; a “float bridge” was used to carry passengers until a replacement could be constructed (Albany Evening Journal 1869: 3; Daily Albany Argus 1869:1). Work to replace the bridge was ongoing as late as 1871 (Daily Albany Argus 1871:4).

The present 184 foot-long Pratt through-truss bridge over the Normanskill is frequently cited as having been constructed in 1880 (Structure 68, Photo 36). While the abutments of the bridge may date to that period or earlier, an inspection report published in 1907 indicates that at some time between June 1904 and June 1906 “bridge No. 4, over the Normanskill, a light iron structure, [was]... replaced with a new steel structure...” (Dunn 1907:349). This may have been undertaken in response to a flood in October 1903, which was recorded as having taken out part of a railroad bridge in this vicinity (Dallas Morning News 1903:2).

A bridge carrying the rail line over South Pearl Street was constructed some time before 1873 (the earliest map it appears on), and was probably an original feature of the line (Bingham 1873). A new bridge was required in 1932, when the course of Route 9W was altered (Structure 91, Photo 50). The old bridge over what is now Old South Pearl Street was maintained for some time, but was eventually partially filled in and modified to serve as a pedestrian tunnel in 1935 (Structure 81, Photo 44).

Several culverts and cattle passes are located along this segment of the rail line. These are an early or original feature associated with the construction of the berm which the tracks pass over. In a 1906 inspection report they were described as “open culverts and cattle passes [which] have I-beam or rail stringers and all have standard floors, in good repair. A few timber stringers yet remain on the...Albany branch...” (Dunn 1907:342-43). These structures retain their original or early I-beam or rail stringers as noted in that report (Structures 63, 67 and 69; Photos 31, 35 and 37).

Plans for the Rockefeller Road “over-grade crossing” were approved in 1906 (Dunn 1907:VLII; 159-60). It was designed by James MacMartin in his capacity as chief engineer of the railroad department of the Delaware & Hudson Co. The bridge was constructed to replace an at-grade crossing of Rockefeller Road over the D&H tracks (MacMartin 1906). Most frequently called a “bedstead double-intersection Warren pony truss” bridge, this form of bridge was commonly used in the period c.1890-1930 for small crossings (Bridgehunter 2014a). An alternate name for this common low-traffic bridge form is a “lattice pony truss” bridge (Bridgehunter 2014b). Typically, as with the Rockefeller Road bridge, the deck assembly has great depth and helps to stiffen the structure. While nineteenth century examples are rare, this bridge type is commonly found in twentieth century examples (Parsons Brinckerhoff 2005: 3-39). Although constructed before the era of bridge standardization in New York State (1908-1929), this particular bridge type had been standardized by the late nineteenth century; the Rockefeller Road bridge type was commonly employed throughout the United States, well into the second quarter of the twentieth century (Bridgehunter 2014a; Mead & Hunt 1999:52-53). The bridge is in a poor state of preservation, with significant section loss and loss of integrity. In 1991 an inspection report gave it a sufficiency rating of 28.8/100, and it was closed some time afterward (Bridgehunter 2014c; Structure 61; Photo 29).

The various railroad-related structures at the west end of the project area chiefly date to the period 1928-1932 (Structures 3, 25, 39 and 62; Photos 3, 19, 24 and 30). Together they mark an upgrade of this segment of the line. Underpasses at Elsmere Avenue and Delaware Avenue were constructed during this period. It is likely that previous to this time these were at-grade crossings. Subsequent to 1905, there was a movement to replace this type of crossing with underpasses or bridges to limit vehicular and pedestrian accidents.

Construction of the New York State Thruway resulted in the creation of two continuous truss-deck bridges over the Normanskill in 1954 (Structures 71 and 72; Photographs 38 and 39).

### *Adjacent Residential and Commercial Development*

Even today the project area passes through a largely rural tract of land. At the western end of the project APE, development of subdivisions began in the vicinity of the hamlet of Elsmere close to the turn of the twentieth century. Single family detached wood-frame dwellings were constructed in the various styles then popular, including bungalow (Structures 15, 20 and 28; Photos 12, 15 and 20), and Colonial Revival (Structures 12, 13, 17 and 18; Photos 10, 11, 13 and 14) styles on Herber Avenue. After initial development in the interwar years, little construction occurred until after 1955. Two expansions of the adjacent subdivision, along West Poplar Drive from c.1955-1970, and West and East Poplar Drive in the middle 1990s were undertaken. These later houses typically partake of the Colonial Revival style, with the older dwellings being smaller “cape” type dwellings (Structures 9, 29 and 30; Photos 8, 21 and 22). Late twentieth century houses within the project APE are typically Colonial Revival houses with jettied second floors and attached garages.

The east end of the project area was, in the nineteenth century, the site of the Hamlet of Kenwood. Named after the adjacent estate, with most of its lands leased from the owner of that property—members of the Rathbone family—Kenwood was the site of several mills and factories, and associated housing. Boice Street was initially part of a gridded street pattern associated with Kenwood, connecting to the already-extant South Pearl Street, which was an earlier road dating to the seventeenth century. Today, with the exception of the houses remaining on South Pearl Street (Structures 70 thru 74; Photos 45 thru 49) and Boice Street (Structure 79; Photo 42), all buildings associated with the hamlet of Kenwood have been destroyed.

Commercial development within the project APE is chiefly confined to the South Pearl Street and Delaware Avenue corridors. Sites at the east end of the project APE, along South Pearl Street, are industrial in nature, and are typically associated with or make use of the port of Albany facilities. All of these commercial interests post-date the 1930s, when South Pearl Street (Route 9W), was altered to its present course. None of the structures in this part of the project APE predate 1965.

At the west end of the project APE, Delaware Avenue, originally laid out as the Delaware Turnpike in the early nineteenth century, is a major commercial spine. Although the site of commercial activity for more than a century, none of the standing commercial structures within the project APE predate 1965. A small memorial park, established in 1964, is located at the western end of the project area (Structure 1; Photo 1).

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Photo 1. Structure 1, [280] Delaware Avenue, constructed 1964, facing north. This structure was determined to be ineligible for listing on the National Register because of significant alterations and additions of recent date.



Photo 2. Structure 2, 298 Delaware Avenue, constructed 1910, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 3. Structure 3, BIN 7025300, RR bridge over Delaware Avenue, constructed 1928, facing northeast. This structure has been determined by a previous survey to be eligible for listing on the National Register.



Photo 4. Structure 4, 33-35 Herber Avenue, constructed 1951, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 5. Structure 5, 31 Herber Avenue, constructed 1945, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 6. Structure 6, 29 Herber Avenue, constructed 1945, facing north. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 7. Structure 7, 27 Herber Avenue, constructed 1937, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 8. Structure 9, 25 Herber Avenue, constructed 1949, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 9. Structure 10, 23 Herber Avenue, constructed 1937, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.



Photo 10. Structure 12, 21 Herber Avenue, constructed 1938, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.





Photo 11. Structure 13, 19 Herber Avenue, constructed 1936, facing north. This structure was determined to be eligible for listing on the National Register under Criterion C.



Photo 12. Structure 15, 17 Herber Avenue, constructed 1935, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.





Photo 13. Structure 17, 13 Herber Avenue, constructed 1937, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.



Photo 14. Structure 18, 11 Herber Avenue, constructed 1936, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.





Photo 15. Structure 20, 9 Herber Avenue, constructed 1932, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.



Photo 16. Structure 21, 7 Herber Avenue, constructed 1921, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 17. Structure 22, 5 Herber Avenue, constructed 1921, facing northwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 18. Structure 24, [5 Elsmere Avenue], constructed c.1910 as a post office, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 19. Structure 25, BIN 7046170, RR bridge over Elsmere Avenue, constructed 1932, facing southeast. This structure has been determined by a previous survey to be eligible for listing on the National Register.



Photo 20. Structure 28, 22 West Poplar Drive, constructed 1920, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.





Photo 21. Structure 29, 24 West Poplar Drive, constructed 1956, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 22. Structure 30, 28 West Poplar Drive, constructed 1955, facing north. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 23. Structure 32, 36 West Poplar Drive, constructed 1960, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 24. Structure 39, Culvert, constructed 1932, facing southeast. This structure was determined to be ineligible for listing on the National Register due to lack of distinguishing characteristics.





Photo 25. Structure 42, Blue stone head wall culvert, constructed c.1870, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 26. Structure 57, Rail overpass, by the American bridge Company, constructed c.1890, facing north. This structure has been determined by a previous survey to be eligible for listing on the National Register.





Photo 27. Structure 58, Pipe and collapsed stone headwall, constructed c.1890, facing northeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 28. Structure 59, Limestone and concrete headwall with iron pipe, constructed c.1890, facing northeast. This structure was determined to be ineligible for listing on the National Register because it lacks distinguishing characteristics.





Photo 29. Structure 61, BIN 2200190, Rockefeller Road bridge, constructed 1906, facing southwest. This structure was determined to be ineligible for listing on the National Register because it lacks distinguishing characteristics and is in poor condition.



Photo 30. Structure 62, Culvert, constructed c.1930, facing northeast. This structure was determined to be ineligible for listing on the National Register because it lacks distinguishing characteristics.





Photo 31. Structure 63, Cattle pass, constructed c.1890, facing northeast. This structure was determined to be eligible for listing on the National Register under Criterion C.



Photo 32. Structure 64, Concrete culvert, constructed c.1930, facing northeast. This structure was determined to be ineligible for listing on the National Register because it lacks distinguishing characteristics.





Photo 33. Structure 65, Wood headwall culvert, constructed c.1950, facing north. This structure was determined to be ineligible for listing on the National Register because it lacks distinguishing characteristics.



Photo 34. Structure 66, Stone headwall, constructed c.1868, facing northeast. This structure was determined to be ineligible for listing on the National Register. While this headwall is intact, the north headwall and approximately 20 feet of the brick arch culvert were recently destroyed during hurricane Irene. The structure no longer retains integrity to its period of construction.





Photo 35. Structure 67, Culvert with stone walls, constructed c.1890, facing southeast. This structure was determined to be eligible for listing on the National Register under Criterion C.





Photo 36. Structure 68, RR Bridge over Normanskill Creek, constructed c.1905, facing southwest. This structure has been determined by a previous survey to be eligible for listing on the National Register.



Photo 37. Structure 69, Small bridge with stone endwalls, constructed c.1880, facing northwest. This structure was determined to be eligible for listing on the National Register under Criterion C.





Photo 38. Structure 71, BIN 5513549 (southbound), constructed 1954, facing southeast. This structure was determined to be eligible for listing on the National Register.



Photo 39. Structure 72, BIN 5512549 (northbound), constructed 1954, facing southwest. This structure was determined to be eligible for listing on the National Register.





Photo 40. Structure 76, Ruins of culvert headwall, constructed c.1880, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 41. Structure 78, Former Academy of the Sacred Heart, South Pearl Street, constructed c.1845, c.1875, facing southeast. This structure has been determined by a previous survey to be eligible for listing on the National Register.





Photo 42. Structure 79, 1 Boice Street, constructed c. 1845, facing southwest. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 43. Structure 80, Blue stone head wall culvert, constructed c.1868, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 44. Structure 81, Old South Pearl Street tunnel, constructed 1935, facing southwest. This structure was determined to be ineligible for listing on the National Register because it lacks distinguishing characteristics.



Photo 45. Structure 83, 812 Old South Pearl Street, constructed c.1910, facing east. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 46. Structure 84, 810 Old South Pearl Street, constructed c.1900, facing southeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 47. Structure 85, 798 Old South Pearl Street, constructed c.1890, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.





Photo 48. Structure 86, 782 Old South Pearl Street, constructed c.1840, facing southeast. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



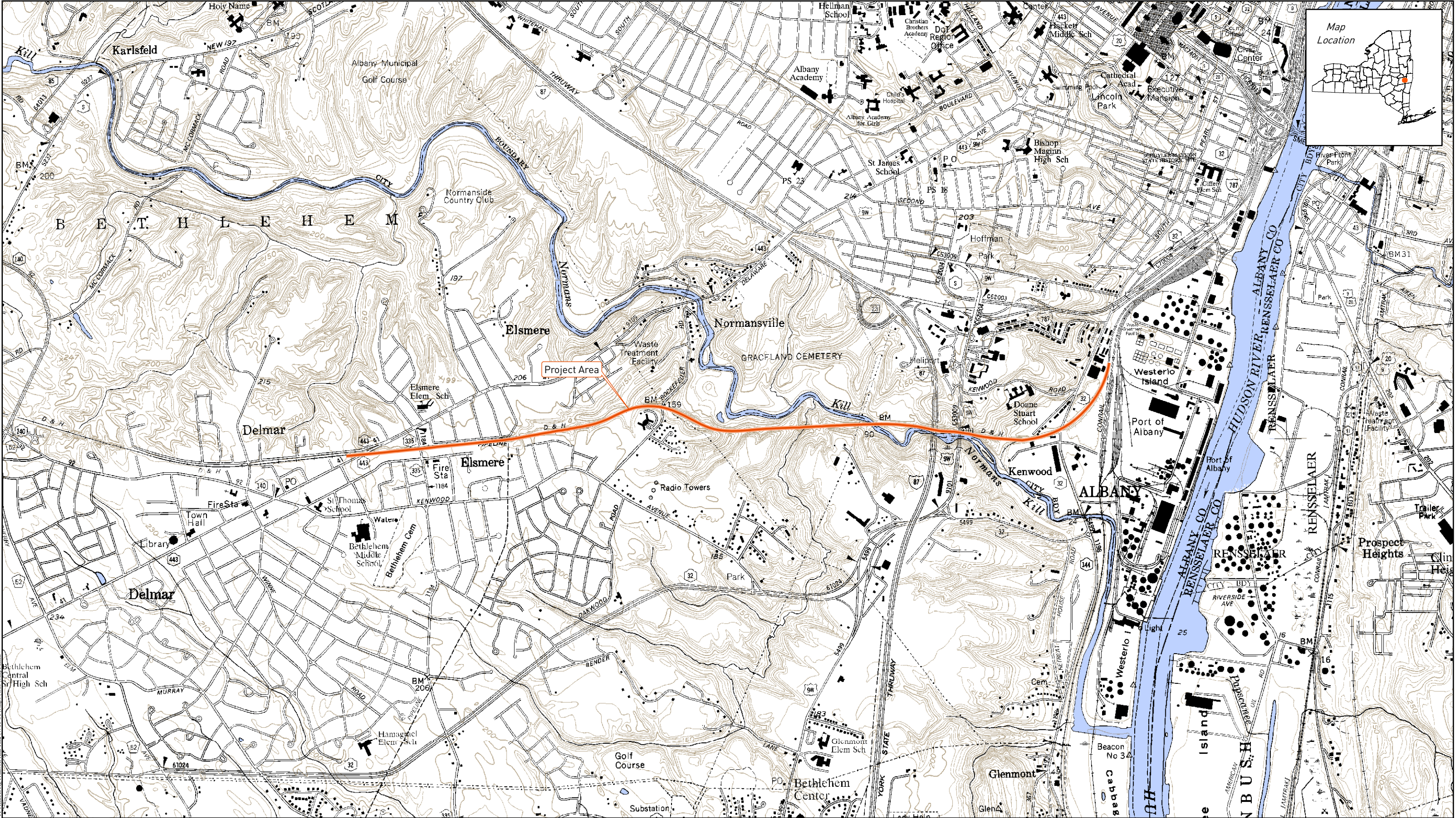
Photo 49. Structure 87, 778 Old South Pearl Street, constructed c.1875, facing south. This structure was determined to be ineligible for listing on the National Register due to loss of integrity to its period of construction.



Photo 50. Structure 91, BIN 7022460, RR Bridge over South Pearl Street, constructed 1932, facing north. This structure was determined to be eligible for listing on the National Register under Criterion C.

## Maps







1,000 0 1,000 2,000 Feet

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Note: Contour interval is 10 feet.

Project Location  
(NYSDOT 1993a, 1993b)

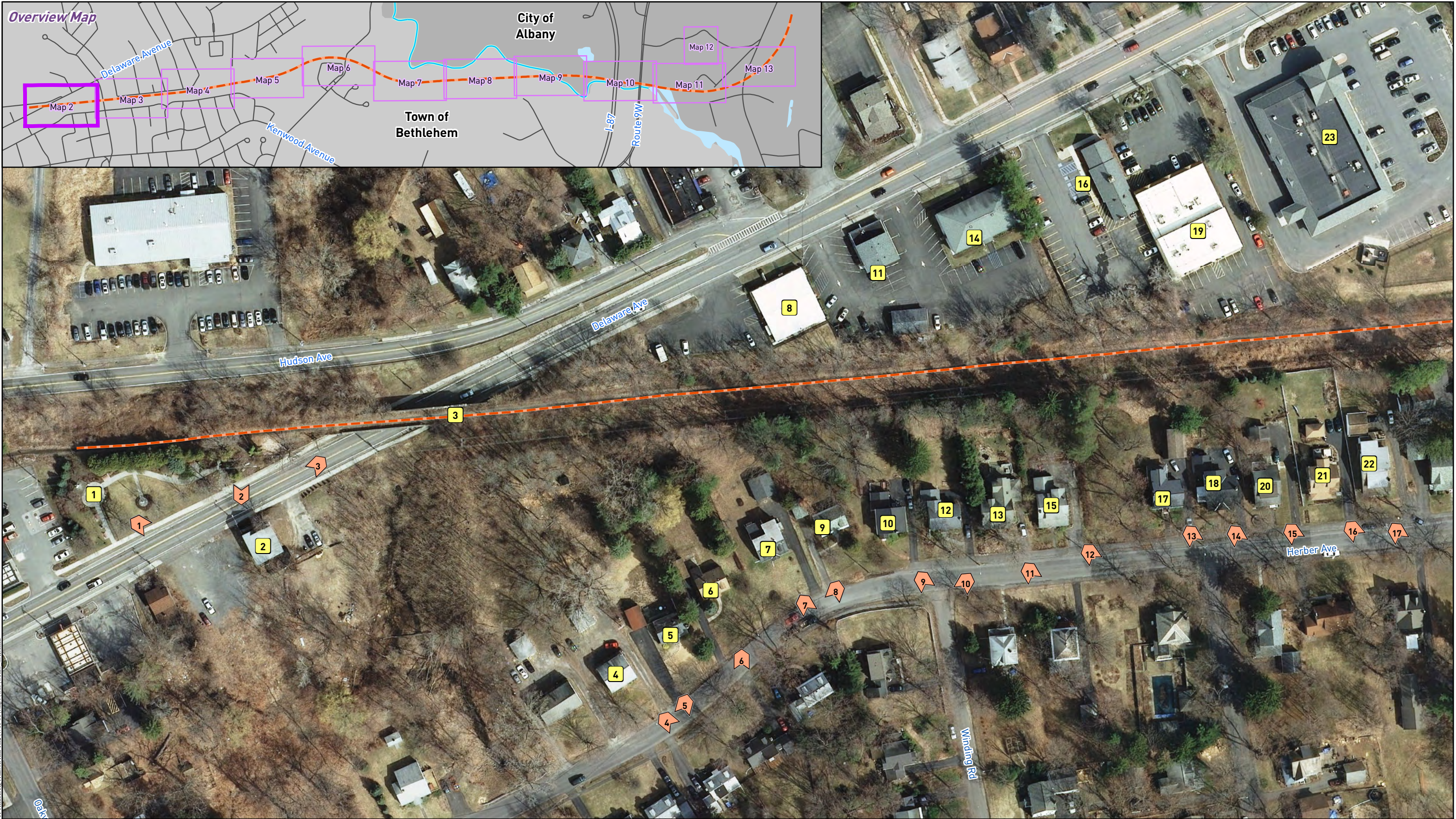


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archeological associates inc

Map 1

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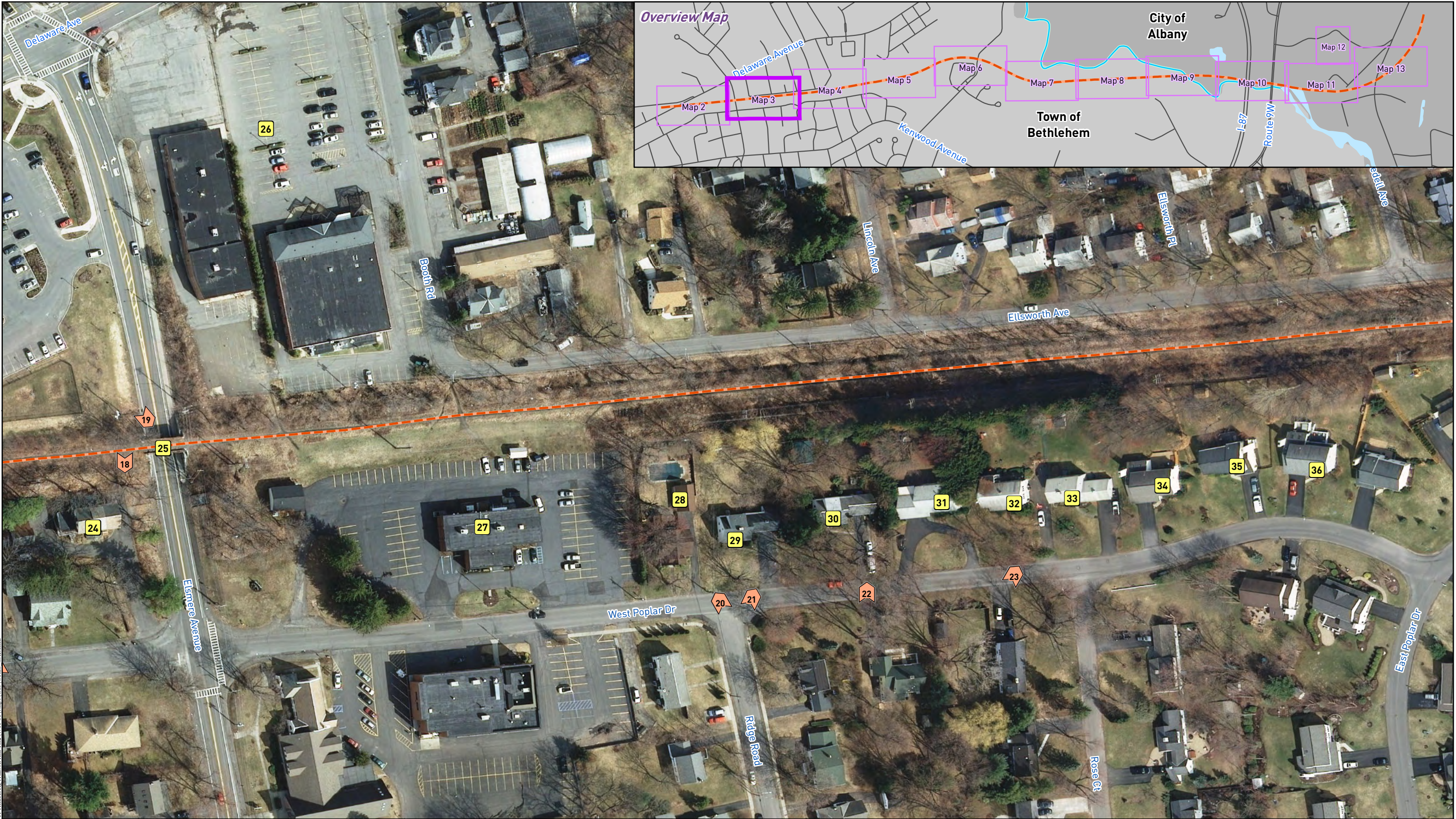
- Legend**
- Structure
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Existing Trail

Project Map  
(Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)







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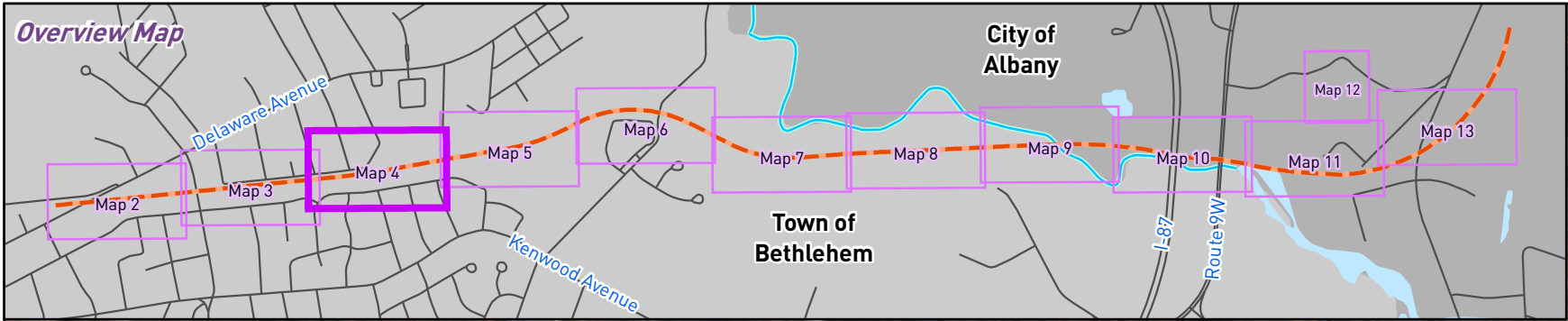
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Existing Trail

Project Map  
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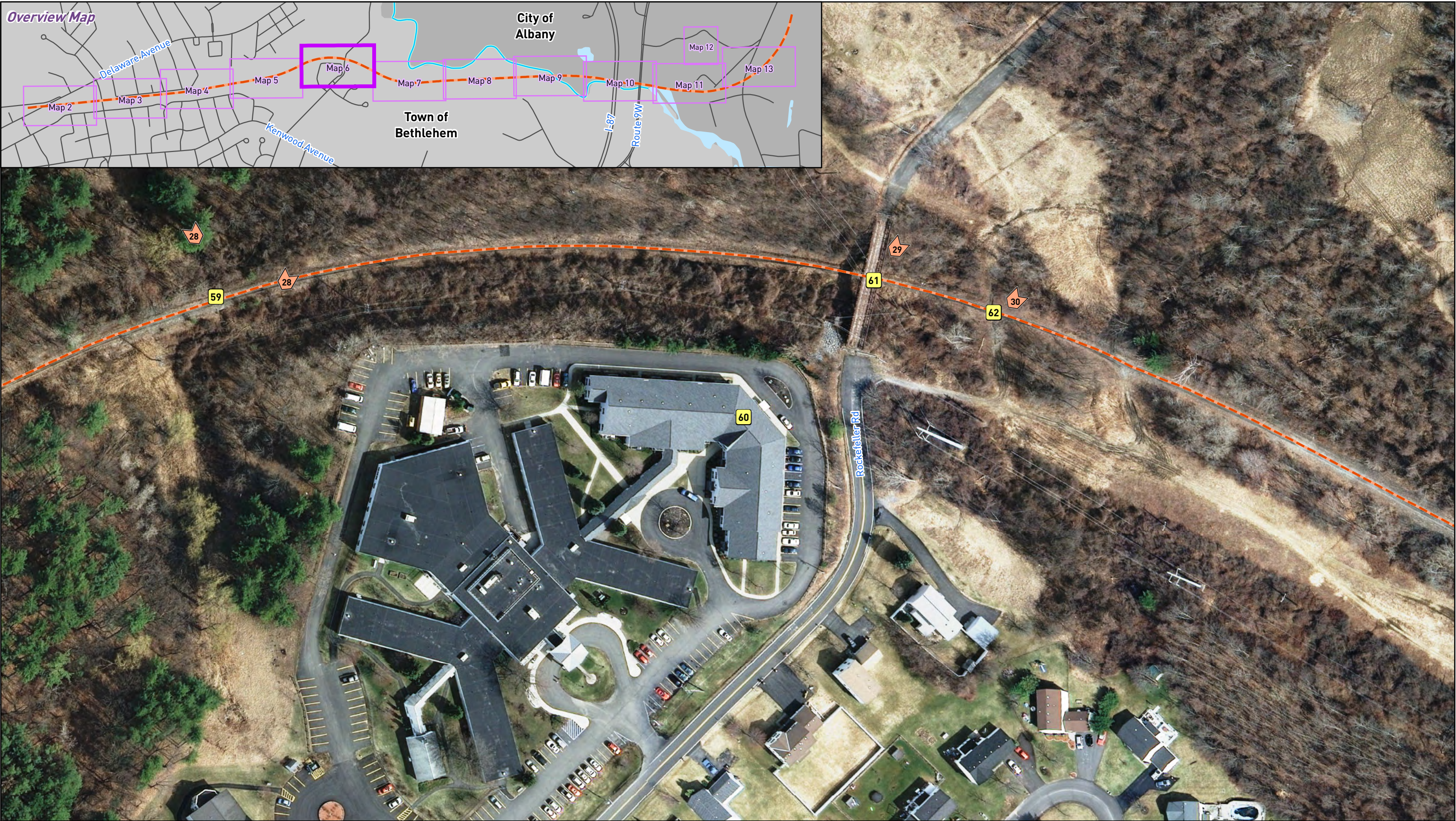
Existing Trail



Project Map  
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 Existing Trail

Project Map  
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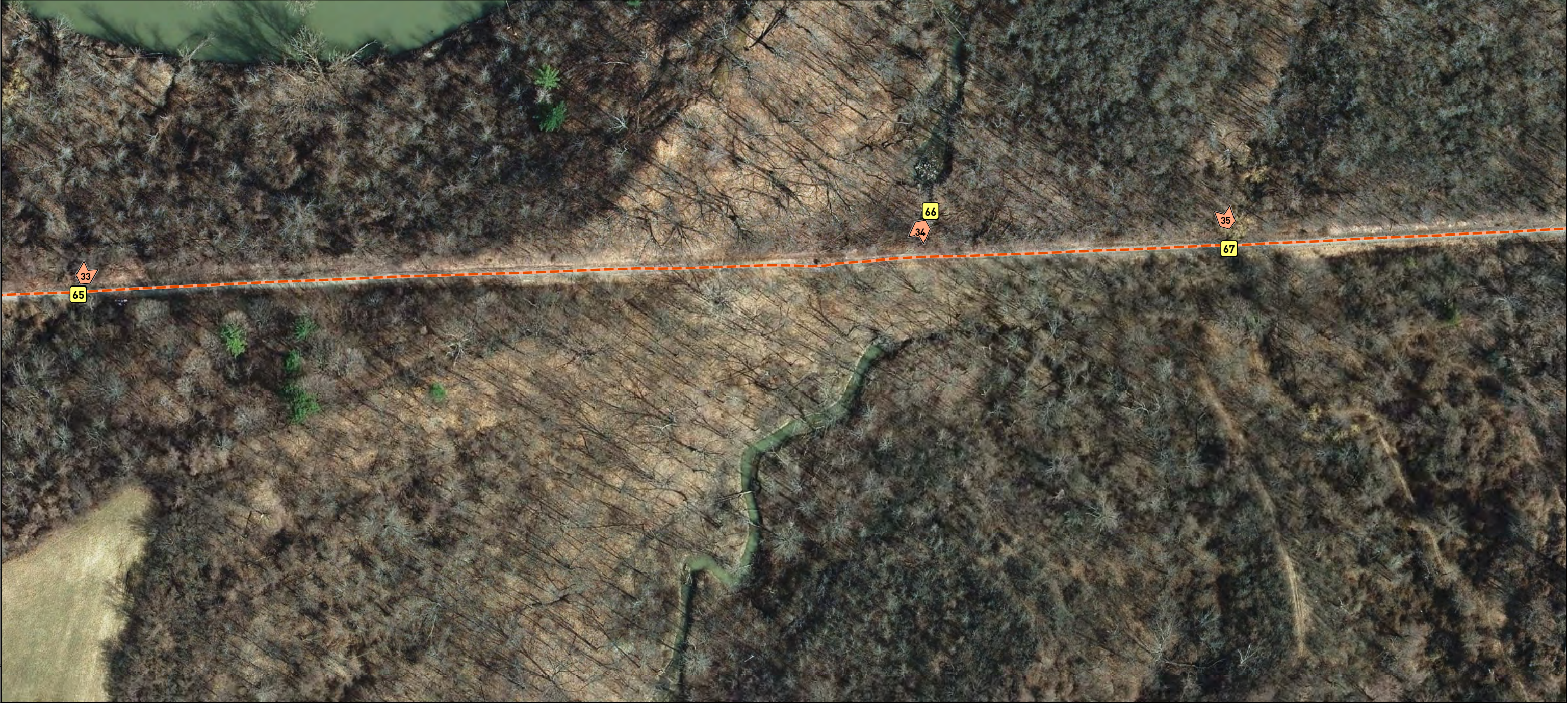
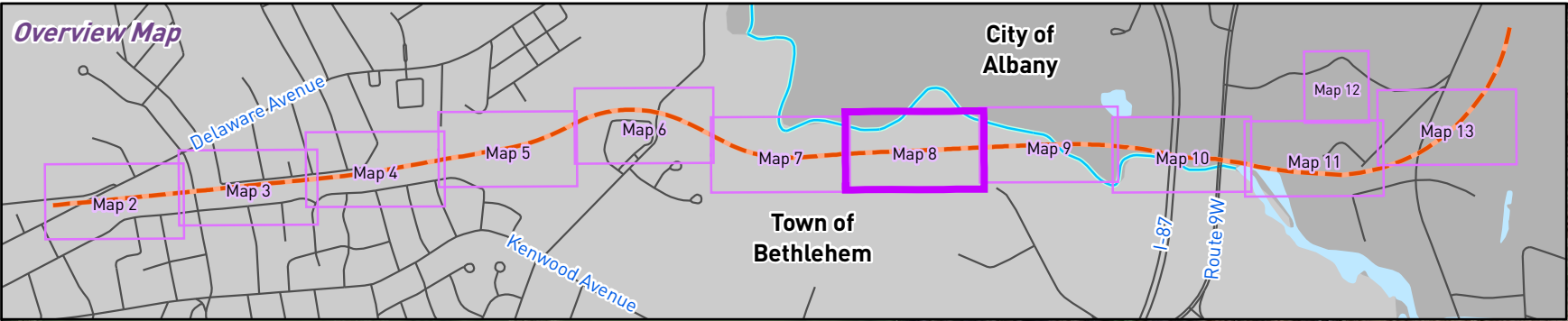
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Existing Trail



Project Map  
(Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)





	50 0 50 100 Feet	<b>Legend</b> Structure Photograph Angle	Existing Trail	
	15 0 15 30 Meters			
Project Map (Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)				<b>Map 8</b>

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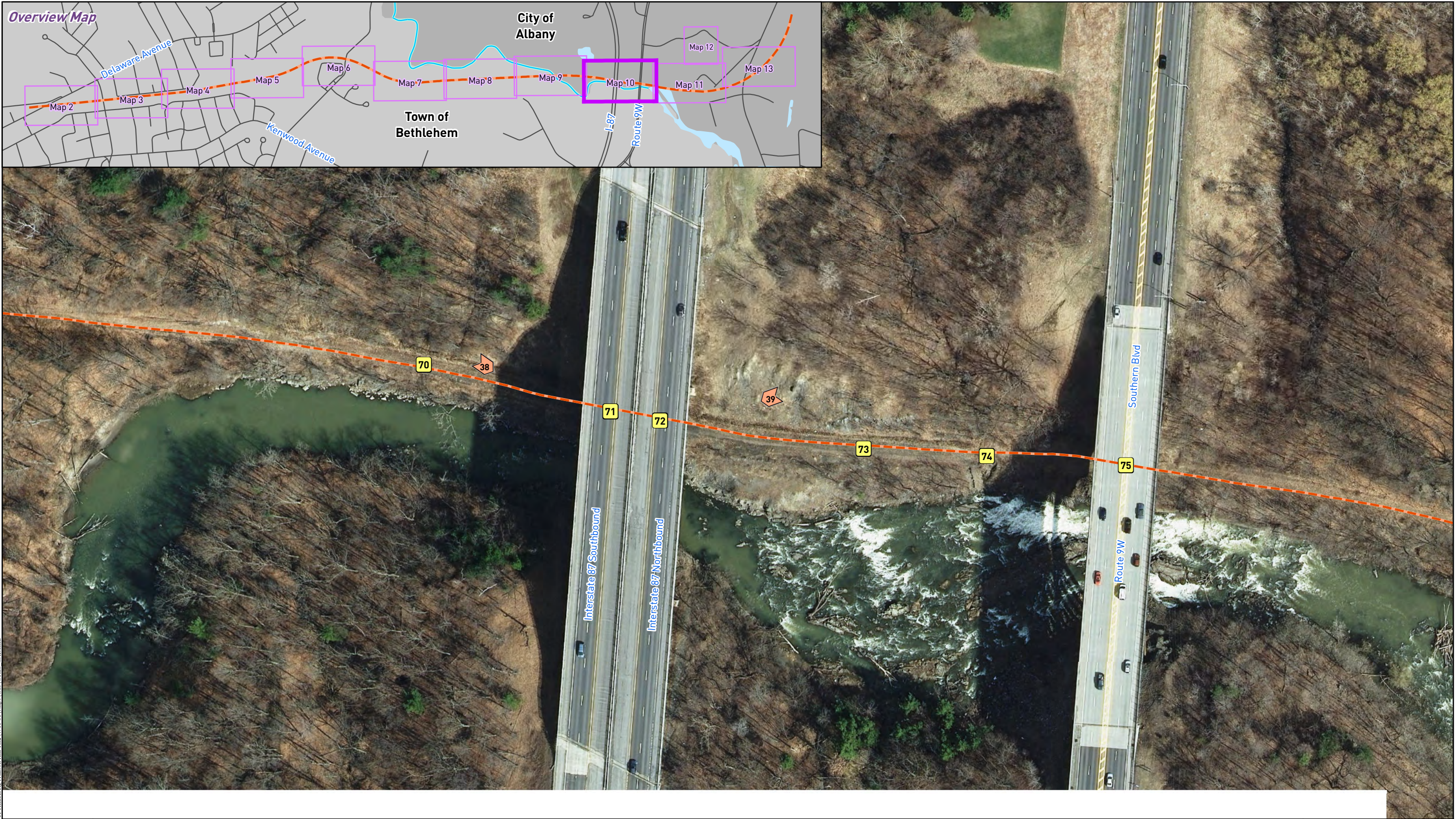
- Legend**
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Existing Trail

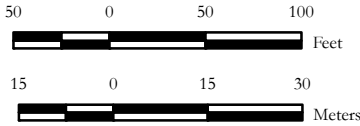
Project Map  
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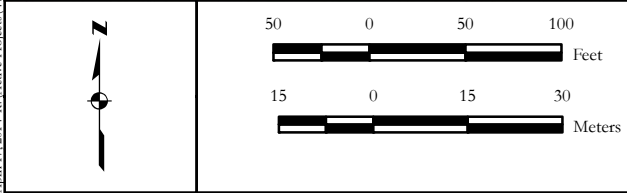
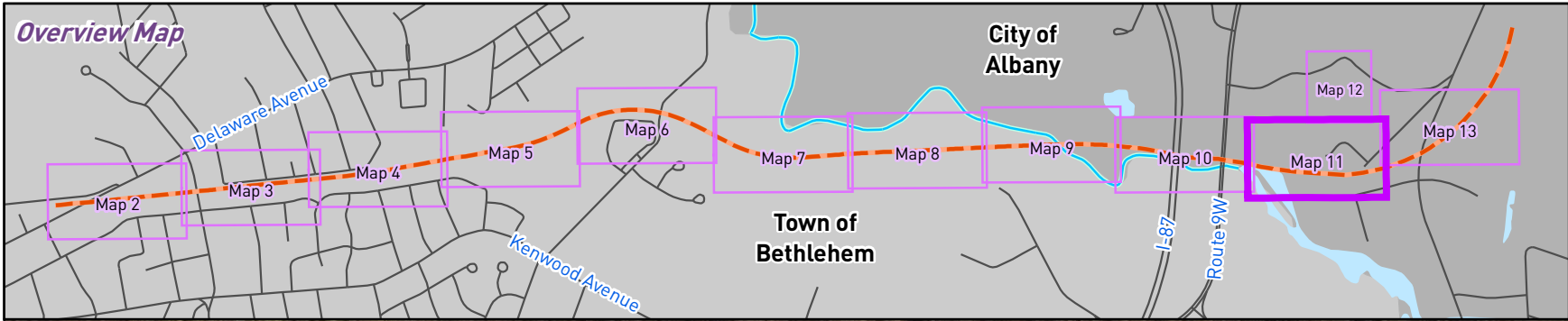
- Legend**
- Structure
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Existing Trail

Project Map  
(Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)







- Legend**
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Existing Trail

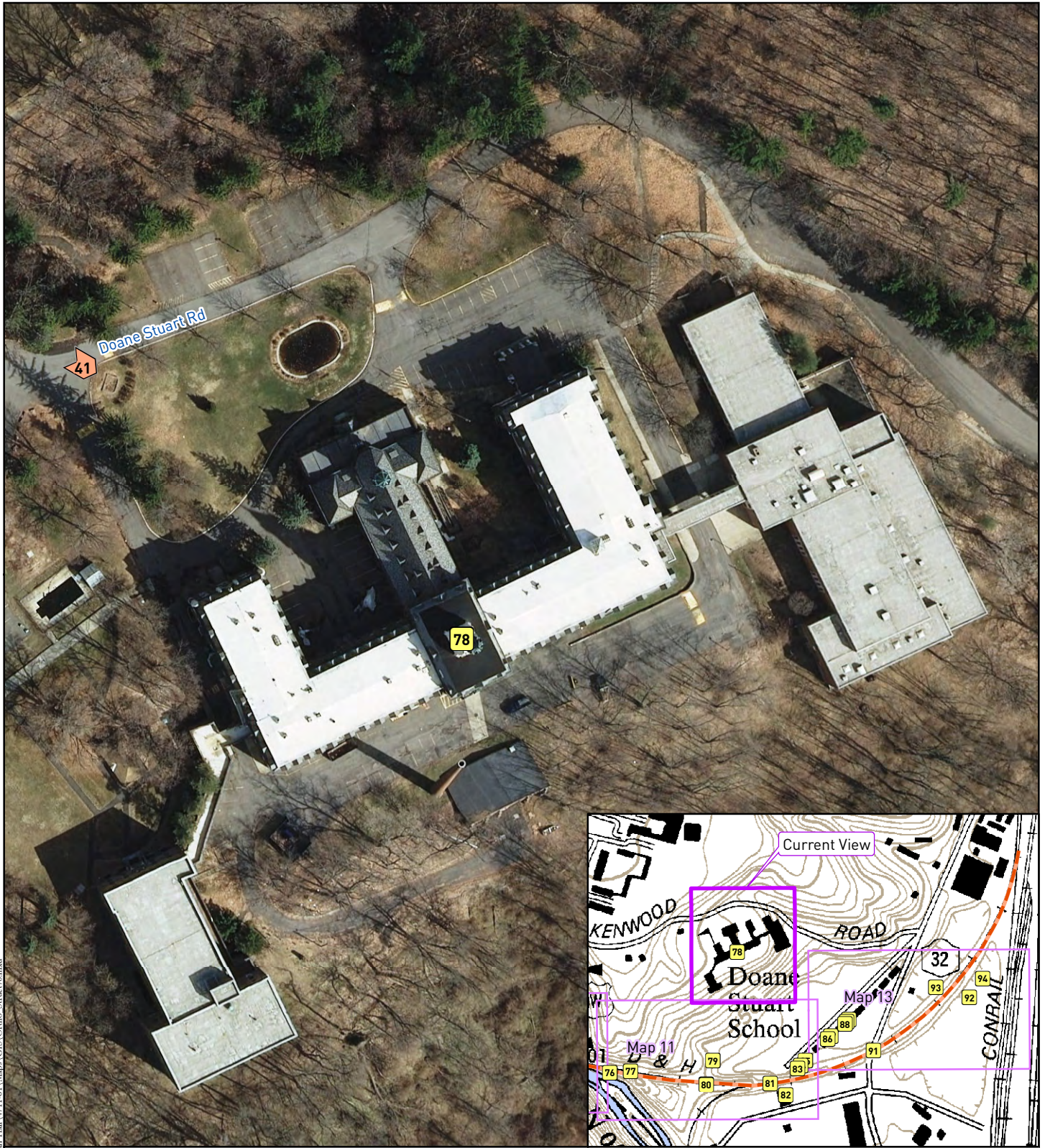
Project Map  
(Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)

**HARTGEN**  
archeological associates inc.

**Map 11**

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# Legend

- Structure
- Photograph Angle

— Existing Trail



50 0 50 100  
Feet

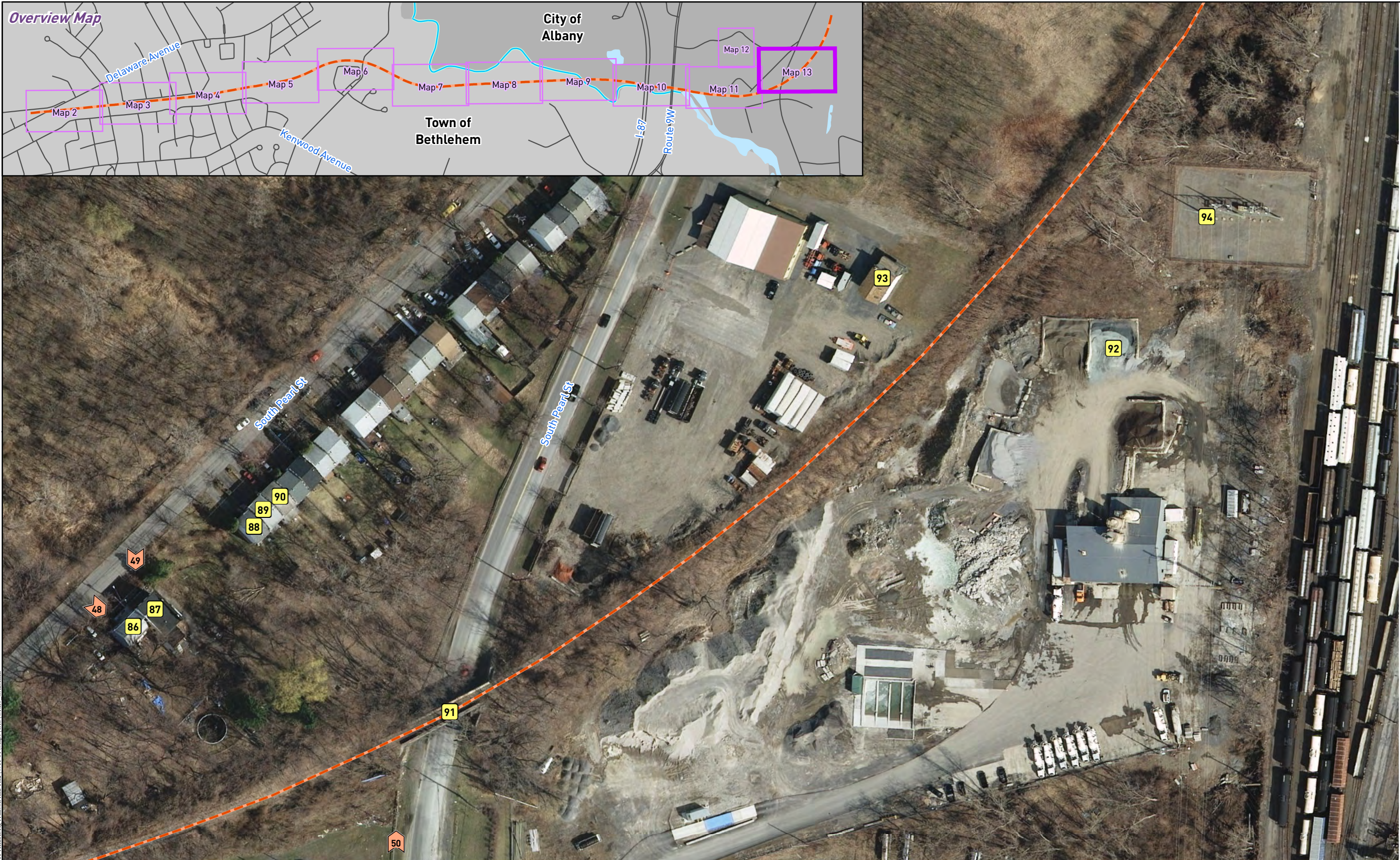
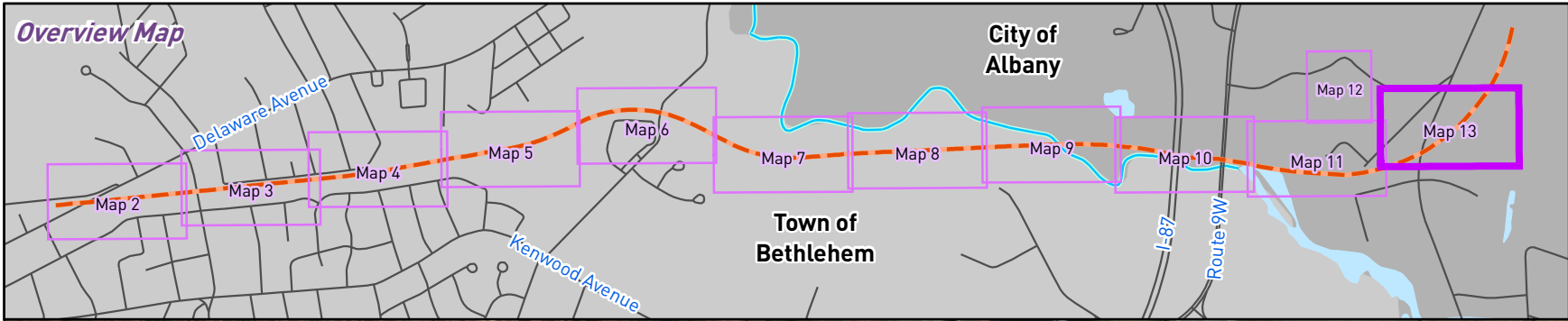
15 0 15 30  
Meters

Strucutre 78, Project Map  
(Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)



Map 12





50 0 50 100 Feet

15 0 15 30 Meters

**Legend**

- Structure
- Photograph Angle
- Existing Trail

Project Map  
(Hartgen 2014; GPI, Inc. 2014; NYSITS 2011)

April 17, 2014 R:\Active Projects\4711 Helderberg Hudson Rail Trail\4711-41\Maps\GIS\Ortho CAD.mxd



## **Appendix I: Building/Structure Inventory Forms**





# HISTORIC RESOURCE INVENTORY FORM

For NYSDOT Projects  
(New York State Education Department Cultural Resources Survey Program Work Scope Specifications,  
Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 23 Herber Avenue (structure 10) Rural Fire Number \_\_\_\_\_  
County Albany Town/City Bethlehem Village/Hamlet: \_\_\_\_\_  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1937

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	<input checked="" type="checkbox"/> wood clapboard	wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation: stone brick ☒ poured concrete concrete block

Alterations: \_\_\_\_\_ Date(s) (if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building: ☒ garage silo privy shed barns  
carriage house other \_\_\_\_\_

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other				

**Photos:**



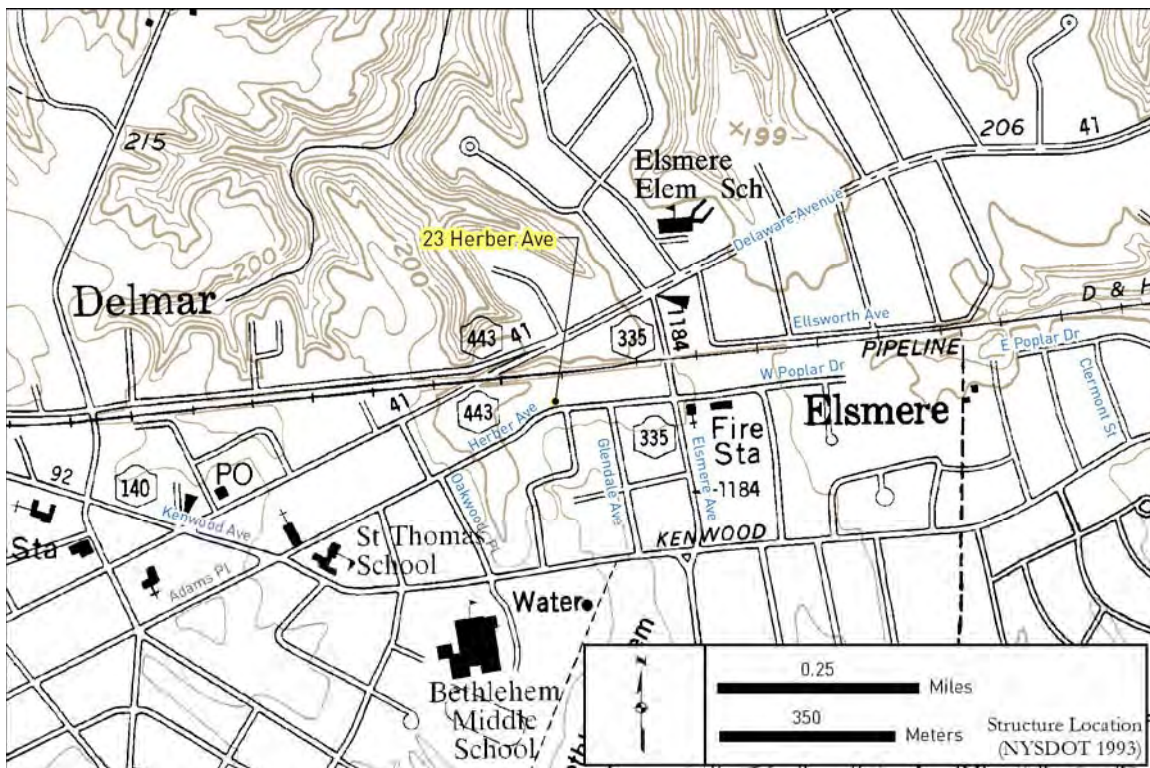
Photo 51. View looking northeast at 23 Herber Avenue.





Photo 52. View looking north-northwest at 23 Herber Avenue.

**Map:**





### **Narrative Description of Property:**

A two-story wood-frame house with end-gable roof constructed in 1937, this dwelling makes use of simplified Colonial Revival detailing popular in the interwar years (1918-1941). The plan is likely of the foursquare type, and features a projecting vestibule and attached rear one-bay garage. Windows are 3-over-6 double hung sash and the house is sheathed with wide clapboards. A dark red brick expressed chimney with yellow brick bands, is a principal design feature of the street elevation. The house occupies a concrete, or parged concrete block foundation.

### **Narrative Description of National Register Eligibility**

An intact representative example of a mid-size suburban dwelling from the interwar years, 23 Herber Avenue is eligible for listing on the National Register under Criterion C.

### **Sources:**

Hubka, Thomas C.

2013    *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002    *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
London: PRC Publishing Company, Ltd.





# HISTORIC RESOURCE INVENTORY FORM

For NYSDOT Projects  
(New York State Education Department Cultural Resources Survey Program Work Scope Specifications,  
Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 21 Herber Avenue (structure 12) Rural Fire Number \_\_\_\_\_  
County Albany Town/City Behtlehem Village/Hamlet: \_\_\_\_\_  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1938

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	<input checked="" type="checkbox"/> wood clapboard	wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation: stone brick ☒ poured concrete concrete block

Alterations: \_\_\_\_\_ Date(s)(if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building: ☒ garage silo privy shed barns  
carriage house other \_\_\_\_\_

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other				

**Photos:**



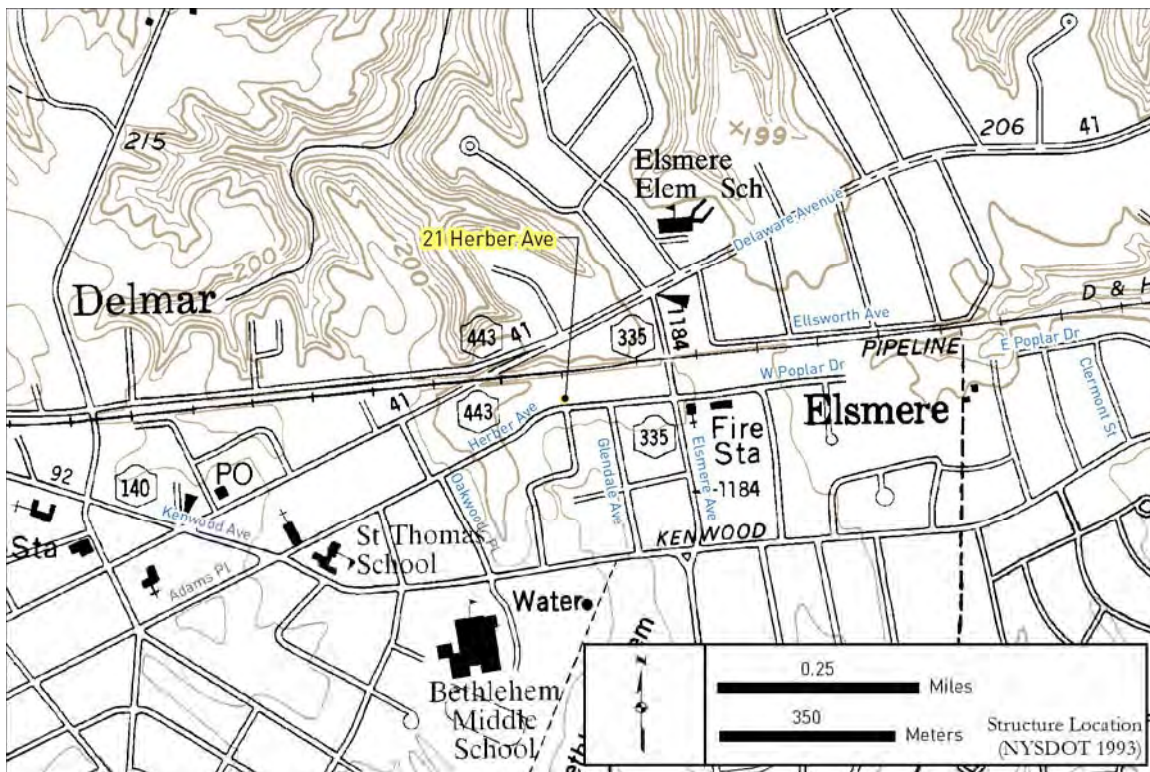
Photo 53. View of 21 Herber Avenue, looking north-northwest.





Photo 54. View of 21 Herber Avenue, looking northeast.

**Map:**





### **Narrative Description of Property:**

A two story side-gable wood-framed center passage house constructed in 1939 with attached one-story garage, this house occupies a concrete or parged concrete block foundation. The first floor of the street elevation is divided into five bays, with the central bay occupied by a door with sidelights, sheltered by a gabled porch. An exposed brick chimney is located on the east elevation. Form and details of the house, including gable-end cornices and the aforementioned porch, make use of the simplified Colonial Revival style popular in the interwar years (1918-1941). Windows are 6-over-1 double hung sash throughout.

### **Narrative Description of National Register Eligibility**

Twenty-one Herber Avenue is an intact representative example of a mid-size Colonial Revival dwelling constructed in the interwar period. It retains all of its period details, and is eligible for listing on the National Register under Criterion C.

### **Sources:**

Hubka, Thomas C.

2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
London: PRC Publishing Company, Ltd.





# HISTORIC RESOURCE INVENTORY FORM

For NYSDOT Projects  
(New York State Education Department Cultural Resources Survey Program Work Scope Specifications,  
Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 19 Herber Avenue (structure 13) Rural Fire Number \_\_\_\_\_  
County Albany Town/City Behlehem Village/Hamlet: \_\_\_\_\_  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1936

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	<input checked="" type="checkbox"/> wood clapboard	wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation: stone brick ☒ poured concrete concrete block

Alterations: \_\_\_\_\_ Date(s) (if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building: ☒ garage silo privy shed barns  
carriage house other \_\_\_\_\_

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other				

**Photos:**



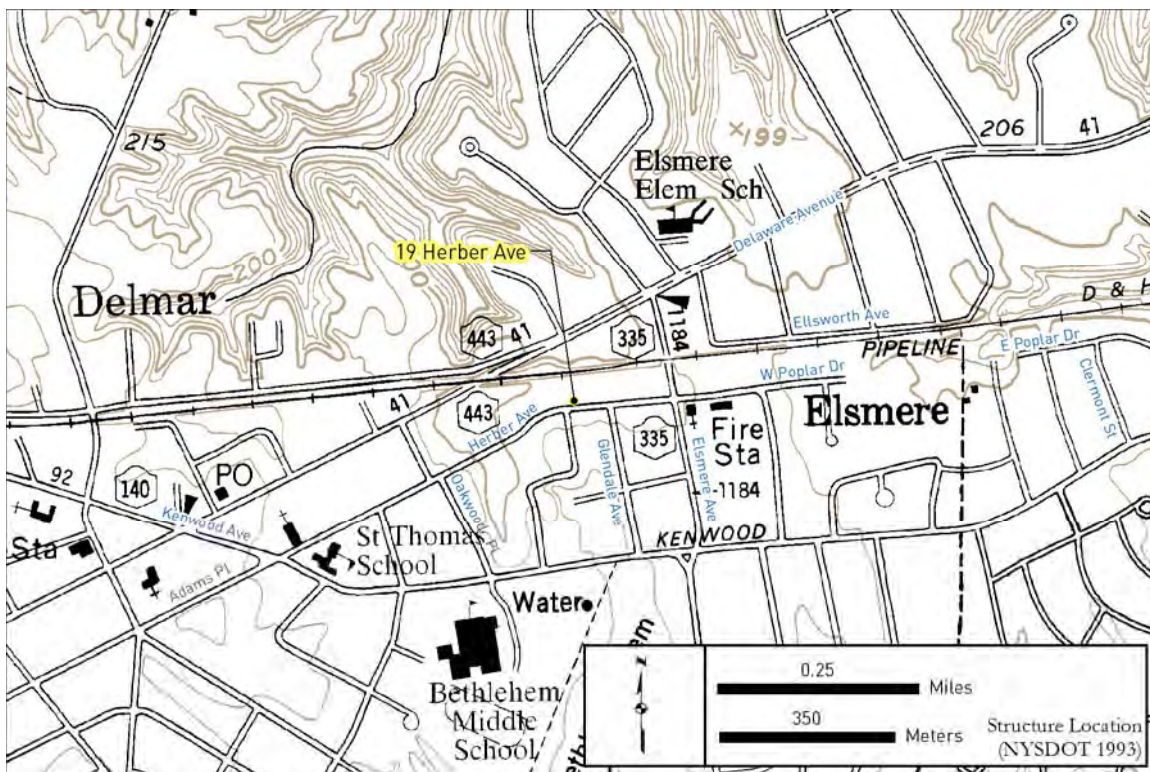
Photo 55. View looking northwest at 19 Herber Avenue.





Photo 56. View looking north-northeast at 19 Herber Avenue.

**Map:**





### **Narrative Description of Property:**

A one-and-one-half story wood-framed house on a concrete or parged concrete block foundation, 19 Herber Avenue has a side gable roof with projecting pavilion at the west end of the street façade. A shed-roofed porch fills the angle made by this feature. An attached garage with shed roof is contemporary to the construction of the house and matches those on adjacent houses apparently constructed by the same builder. This house is of an unknown plan type, but is likely two rooms deep, and may have a central stair hall. The house retains its original siding and 6-over-1 double hung sash, as well as all other period details. An exposed brick chimney is a principal feature on the west elevation. In the Colonial Revival style, this house was built in 1936. One minor change was noted: the replacement of one window with a small projecting glazed window in what appears to be the kitchen.

### **Narrative Description of National Register Eligibility**

An intact representative example of a Colonial Revival dwelling constructed in the interwar years (1918-1941), 19 Herber Avenue is eligible for listing on the National Register under Criterion C.

### **Sources:**

Hubka, Thomas C.

2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
London: PRC Publishing Company, Ltd.





# HISTORIC RESOURCE INVENTORY FORM

For NYSDOT Projects  
(New York State Education Department Cultural Resources Survey Program Work Scope Specifications,  
Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 17 Herber Avenue (structure 15) \_\_\_\_\_ Rural Fire Number \_\_\_\_\_  
County Albany Town/City Bethlehem Village/Hamlet: \_\_\_\_\_  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1935

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	wood clapboard	<input checked="" type="checkbox"/> wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation:	stone	brick	poured concrete	concrete block
-------------	-------	-------	-----------------	----------------

Alterations: \_\_\_\_\_ Date(s) (if known): \_\_\_\_\_

Condition: excellent good fair deteriorated

Associated Building:	<input checked="" type="checkbox"/> garage	silo	privy	shed	barns
carriage house	other _____				

Landscape Features: gardens pond mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other				

**Photos:**



Photo 57. View looking northwest at 17 Herber Avenue.





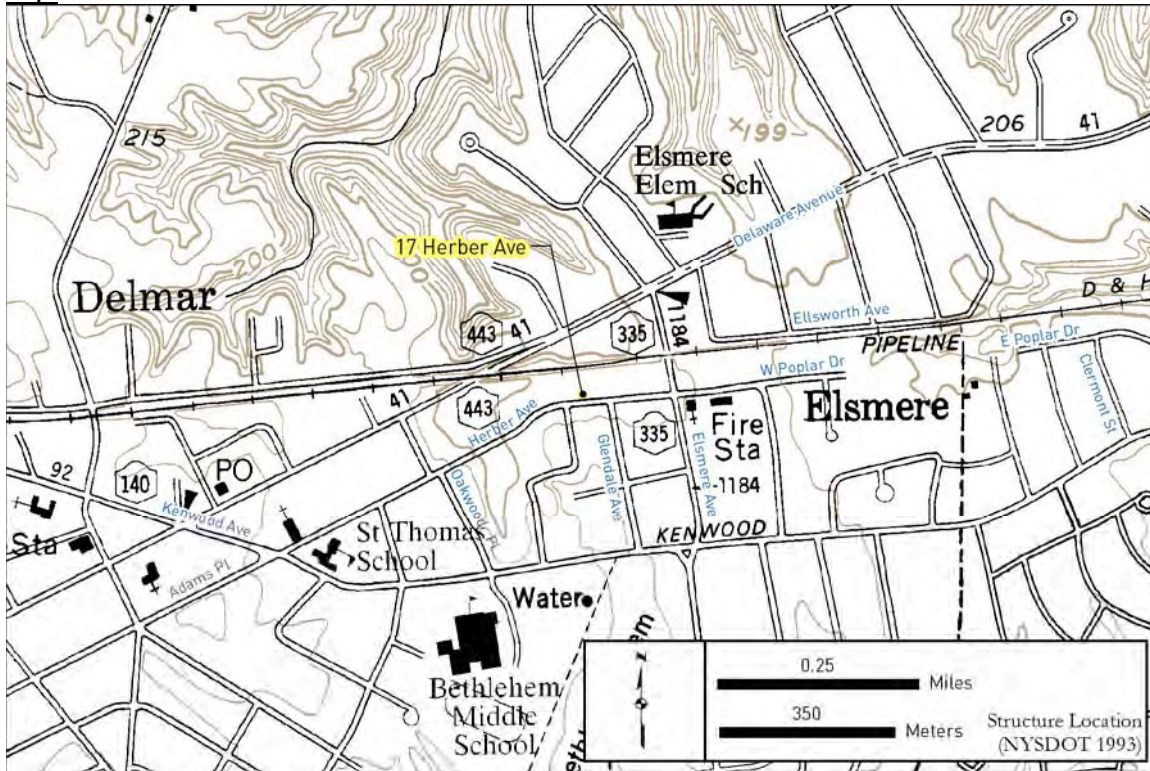
Photo 58. View looking northeast at 17 Herber Avenue.



Photo 59. View looking southwest at 17 Herber Avenue.



**Map:**



**Narrative Description of Property:**

Constructed in 1935, this bungalow style home is typical of house designs offered by Wardway Homes and Sears, Roebuck & Co in the 1920s and 1930s. It is a one-and-a-half story side gable roofed wood framed dwelling, sheathed in shingles and clapboards. The broad roof has exposed rafter tails and wide eaves supported on trusses, and covers a porch extending across the street elevation. The porch roof is supported on clustered columns on flared shingled plinths. The porch was enclosed at an early date (or perhaps when the house was initially constructed); triple windows and the central door with sidelights are all capped with glazed fan lights. Sash used throughout the house are of the 6-over-1 double hung type, including the two double windows in the large shed-roofed dormer which is the principal feature of the roof on the street elevation. Other features of the house include a projecting box window on the east elevation, a yellow-brick exposed chimney on the west elevation, and a two story bay window on the north elevation. A contemporary one-story wood framed garage is located close to the house in the back yard.

**Narrative Description of National Register Eligibility**

An intact representative example of a bungalow style house, 17 Herber Avenue retains all of its original details and is eligible for listing on the National Register under Criterion C.

**Sources:**

Hubka, Thomas C.

2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
London: PRC Publishing Company, Ltd.





# HISTORIC RESOURCE INVENTORY FORM

For NYSDOT Projects  
(New York State Education Department Cultural Resources Survey Program Work Scope Specifications,  
Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 13 Herber Avenue (structure 17) Rural Fire Number \_\_\_\_\_  
County Albany Town/City Bethlehem Village/Hamlet: \_\_\_\_\_  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1937

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	wood clapboard	<input checked="" type="checkbox"/> wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation:	stone	brick	poured concrete	<input checked="" type="checkbox"/> concrete block
-------------	-------	-------	-----------------	--

Alterations: \_\_\_\_\_ Date(s) (if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building:	<input checked="" type="checkbox"/> garage	silo	privy	shed	barns
carriage house	other _____				

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other _____				

**Photos:**



Photo 60. View looking northwest at 13 Herber Avenue.





Photo 61. View looking east-northeast at 13 Herber Avenue.





**THE VAN JEAN** is an unusually well arranged Dutch Colonial house. It has many special features not generally found in houses of this price. It has a charming entrance that gives an atmosphere of welcome. It has Colonial windows with divided lights above and one light below. Add to this the white siding and contrasting red or green roof with the red brick chimney and you have a home that is sure to charm the most critical.

The interior is cleverly planned. While it has the latest conveniences, the price is unusually low. Why? Because of careful planning and no wasted space. If a house of this size meets with your requirements, you will make no mistake in selecting the Van Jean.

#### FIRST FLOOR

**The Reception Hall.** Entry into the reception hall reveals the splendid character of this Dutch Colonial home. An open stairway leads to the second floor. To the left of stairway is a coat closet with pole for coats and a hat shelf, accommodating a large number of guests' clothes. A grandfather clock can be set along the left wall. On each side of this reception hall are wide openings with French doors, giving an excellent view of the large living room and the dining room on the opposite side.

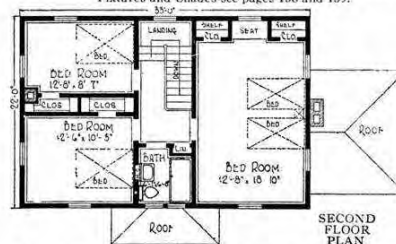
**The Living Room.** To the right of hall you enter the living room, which is of the proper proportions to accommodate its furnishings to the best advantage. It is unusually large for a house of the size of the Van Jean. See the floor plan. Almost directly opposite the wide opening with French doors is a brick mantel and fireplace which is the central feature. All furniture may be grouped to your liking, as the space is ample. Windows on three sides afford plenty of light and air, making a cheery room.

**The Sun Room.** A French door opens into the sun room from the living room if P3267A is ordered. Seven large windows flood this room with sunshine. Tastefully furnished and draped, it can be made the chosen spot in winter or summer.



Can be built on a lot 50 feet wide with sun room, and on a lot 40 feet wide without sun room.

This house can be built with the rooms reversed. See page 3.



**Honor Bill**  
**The Van Jean**  
"Already Cut" and Fitted  
P3267A Price with Sun Room, \$2,887.00  
P3267B Price without Sun Room, \$2,636.00

#### What Our Prices Include

At the prices quoted we will furnish all the material to build this six-room colonial house, consisting of:  
Lumber; Lath;  
Roofing, Oriental Slate Surfaced Shingles. Guaranteed for 17 years.  
Siding, 1/2x6-inch Clear Cypress or Clear Red Cedar, Bevel.  
Framing Lumber, No. 1 Quality Douglas Fir or Pacific Coast Hemlock.  
Flooring, 1/2x2 1/4-inch Clear Oak throughout except Kitchen and Bathroom which are 1/2x2 1/4-inch Clear Maple.  
Porch Ceiling, Clear Grade Douglas Fir or Pacific Coast Hemlock.  
Finishing Lumber; High Grade Millwork (see pages 104 and 105).  
Interior Doors, One-Panel Sub-Frame Design of Clear Fir.  
Trim, Back Band Style, of Beautiful Grain Douglas Fir or Yellow Pine; Birch Stair Treads, Newell and Rail; Mantel; Scent.  
Kitchen Cabinets; Medicine Case.  
Windows of California Clear White Pine;  
Heavy Waterproof Building Paper; Sash Weights;  
Eaves Trough and Down Spout;  
Narcissus Design Hardware (see page 116).  
Paint for Three Coats Outside Trim and Siding;  
Five-Coat Enamel Finish for Trim; Mahogany Stain, Shellac, Two Coats of Varnish for Doors, Stair Treads, Rail, and Newell.  
Wood Filler and Two Coats of Varnish for Oak Floors;  
Two Coats of Varnish for Maple Floors;  
Complete Plans and Specifications.  
Built on a concrete and brick foundation and excavated under entire house.  
We guarantee enough material to build this house. Prices do not include cement, brick or plaster.  
See description of "Honor Bill" Houses on pages 12 and 13.

#### OPTIONS

4-in-1 Style Oriental Asphalt Slate Surface Strip Shingles, guaranteed for seventeen years, instead of Oriental Asphalt Shingles for roof \$5.00 less for P3267A and \$5.00 for P3267B.  
Sheet Plaster and Plaster Finish, to take the place of wood lath, with sun room, \$168.00; without sun room, \$160.00 extra. See page 110.  
Storm Doors and Windows, with sun room, \$87.00 extra; without sun room, \$71.00.  
Screen Doors and Windows, galvanized wire, with sun room, \$53.00 extra; without sun room, \$43.00.  
For prices of Plumbing, Heating, Wiring, Electric Fixtures and Shades see pages 138 and 139.

For Our Easy Payment Plan See Page 144—For Our Information Blank See Page 141

Page 30

See Interior Views of The Van Jean Home on opposite Page

P602

Figure 1. This image, taken from a 1928 Sears, Roebuck & Co. catalog, likely represents the same dwelling without customizations (<http://www.searsarchives.com/homes/1927-1932.htm>, accessed 29 April 2014).



The map shows the Elsmere Fire Station (1184) located on Elsmere Ave. Key features include:
 

- Geography:** Contour lines indicating elevation, with labels for 200, 215, and 206 feet.
- Roads:** Delaware Avenue, Herber Ave, Ellsworth Ave, W Poplar Dr, E Poplar Dr, and Clearmont St.
- Landmarks:** Elsmere Elem Sch, Bethlehem Middle School, St Thomas School, and a Post Office (PO).
- Infrastructure:** A pipeline running horizontally across the middle of the map.
- Scale and Orientation:** A scale bar showing 0.25 miles and 350 meters, and a north arrow pointing upwards.
- Other Labels:** "Delmar" to the west, "Elsmere" to the east, and "KENWOOD" near the fire station.

A two-story wood-framed house, the second story of which is disguised by a combination of a gambrel roof with shed-roofed dormers extending across both of its long elevations. The house, built in 1937, appears to match the "Van Jean" model (but reversed) offered by Sears, Roebuck & Co., by 1928, with several changes. These differences, which likely represent customizations by the original client, include substitution of 9-over-1 windows for 8-over-1's, changes to the second floor dormer fenestration pattern, changes to the design of the covered entry, and construction of an ell at the back of the house. This last feature appears to be contemporary with the rest of the dwelling. This Colonial Revival house occupies a rusticated concrete block foundation and is sheathed with shingles.

This house retains all of its period detailing and is eligible for listing on the National Register under Criterion C, as an intact representative example of a kit-built home, constructed with customizations, in the interwar years.

Hubka, Thomas C.  
2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses*.  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.  
2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture*.  
London: PRC Publishing Company, Ltd.

Sears Homes  
2014 Sears Homes 1927-1932. <http://www.searsarchives.com/homes/1927-1932.htm>, accessed 29 April 2014





# HISTORIC RESOURCE INVENTORY FORM

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Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 11 Herber Avenue (structure 18) Rural Fire Number \_\_\_\_\_  
County Albany Town/City Bethlehem Village/Hamlet: \_\_\_\_\_  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1936

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	<input checked="" type="checkbox"/> wood clapboard	wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation: stone brick poured concrete ☒ concrete block

Alterations: \_\_\_\_\_ Date(s)(if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building:	<input checked="" type="checkbox"/> garage	silo	privy	shed	barns
carriage house	other _____				

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other				

**Photos:**



Photo 62. View looking north-northwest at 11 Herber Avenue.





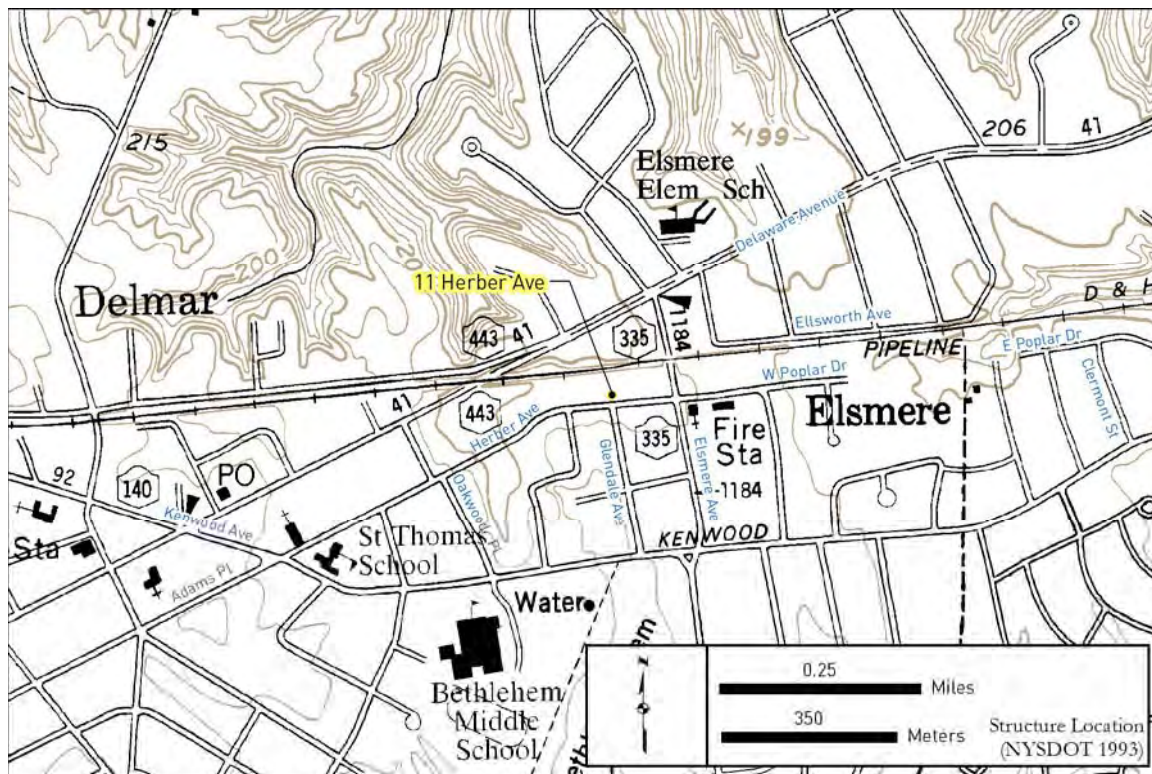
Photo 63. View looking northeast at 11 Herber Avenue.



Photo 64. View looking south at 11 Herber Avenue and its associated garage.



**Map:**



**Narrative Description of Property:**

A two-story wood frame house with side-gable roof, occupying a rusticated concrete block foundation. This four-bay wide dwelling has clapboard siding; a substantial shed-roofed enclosed porch with gabled entry bay extends across the front of the house. It and the enclosed sunroom attached to the west elevation appear to be original features. An exposed dark red brick chimney is the principal feature of the east elevation. The house retains its original 9-over-1 double hung sash, paneled shutters, and all other period details. A one-story wood-framed detached garage with clipped-gable roof is located to the north of the house, and shares many of its Colonial Revival details.

**Narrative Description of National Register Eligibility**

An intact representative example of a mid-size Colonial Revival dwelling constructed in the interwar years (1918-1941), this house retains all of its original features. It is eligible for listing on the National Register under Criterion C.

**Sources:**

Hubka, Thomas C.

2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
London: PRC Publishing Company, Ltd.





# HISTORIC RESOURCE INVENTORY FORM

For NYSDOT Projects  
(New York State Education Department Cultural Resources Survey Program Work Scope Specifications,  
Section I.C. (January 1998))



NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_

Address or Street Location 9 Herber Avenue (Structure 20) Rural Fire Number \_\_\_\_\_

County Albany Town/City Bethlehem Village/Hamlet: Elsmere

Original use Residence Current use Residence

Architect/Builder, if known \_\_\_\_\_ Construction Date: 1932

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	<input checked="" type="checkbox"/> wood clapboard	wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	<input checked="" type="checkbox"/> other: <u>stucco</u>				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation:	stone	brick	poured concrete	<input checked="" type="checkbox"/> concrete block
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Alterations: \_\_\_\_\_ Date(s)(if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building:	<input checked="" type="checkbox"/> garage	silo	privy	shed	barns
carriage house	other _____				

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 25 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other				

**Photos:**



Photo 65. View looking northeast at 9 Herber Avenue.





Photo 66. View looking northwest, showing relationship between house and garage.



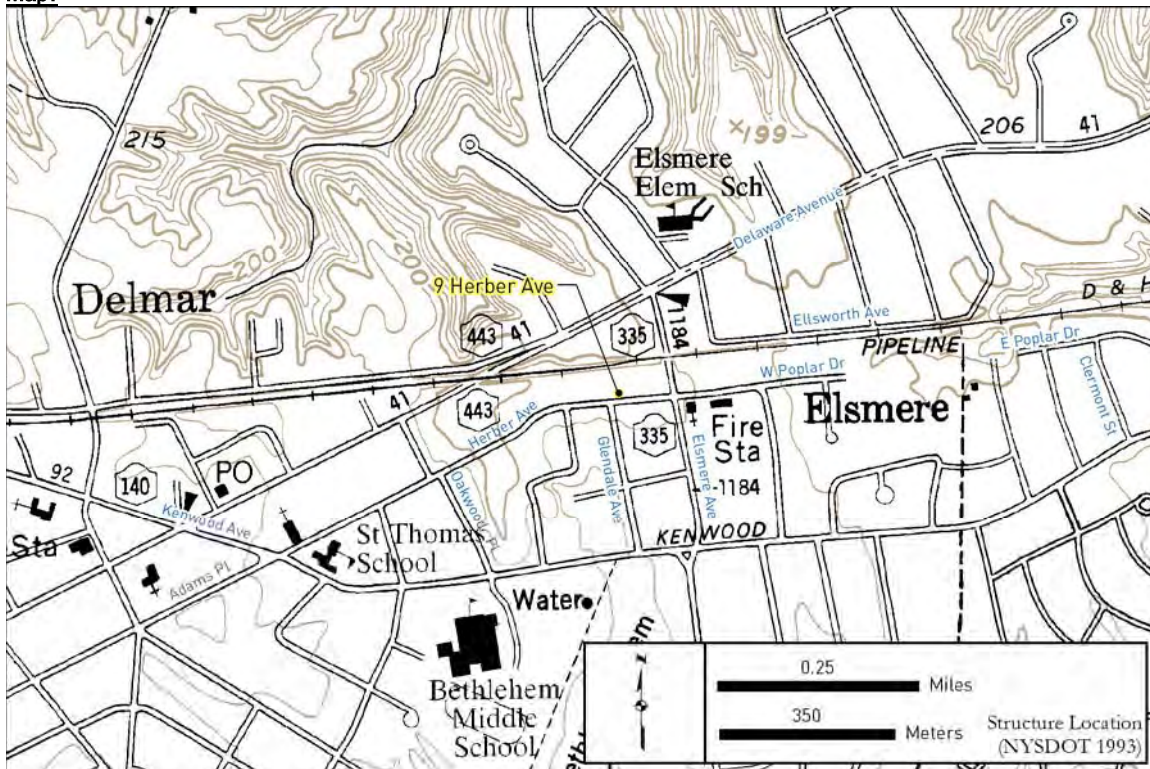
Photo 67. View northwest showing garage.





Photo 68. View southwest.

**Map:**





### **Narrative Description of Property:**

A one-and-one half story wood frame bungalow style home of the type offered by Wardway Homes (Montgomery Ward & Co.) or Sears, Roebuck & Co. in the 1920s and early 1930s. The house is covered by a broad side-gable roof which extends over a porch on the street elevation, the roof being supported on three truncated pyramidal columns. The foundation is comprised of rusticated concrete blocks. The house and its matching garage retain their original rough-cast treatment on the first floor, and shingles above the first floor windows, on both buildings. The house and garage share design details including the aforementioned exterior sheathing materials, exposed rafters at their eaves, and shed-roofed dormers featuring multiple windows. The house and garage are almost entirely intact to their period of construction, the sole exception being the replacement of one window with a casement sash on the east elevation.

### **Narrative Description of National Register Eligibility**

This house is eligible for listing on the National Register under Criterion C; it is an intact representative example of a kit home constructed in the interwar years.

### **Sources:**

Hubka, Thomas C.

2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
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NYS OFFICE OF PARKS, RECREATION  
& HISTORIC PRESERVATION  
P.O. BOX 189, WATERFORD, NY 12188  
(518) 237-8643

OFFICE USE ONLY

USN:

## IDENTIFICATION

Property name (if any) \_\_\_\_\_  
Address or Street Location 22 West Poplar Drive (structure 28) Rural Fire Number \_\_\_\_\_  
County Albany Town/City Bethlehem Village/Hamlet: Elsmere  
Original use Residence Current use Residence  
Architect/Builder, if known \_\_\_\_\_ Construction Date\* 1920

## DESCRIPTION

Please check those which are applicable

Exterior Walls:	<input checked="" type="checkbox"/> wood clapboard	wood shingle	vertical boards	plywood	stone
brick	poured concrete	concrete block	vinyl siding	aluminum siding	
cement-asbestos	other: _____				

Roof: ☒ asphalt, shingle asphalt, roll wood shingle metal slate

Foundation:	stone	brick	poured concrete	<input checked="" type="checkbox"/> concrete block
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Alterations: \_\_\_\_\_ Date(s)(if known): \_\_\_\_\_

Condition: ☒ excellent good fair deteriorated

Associated Building:	<input checked="" type="checkbox"/> garage	silo	privy	shed	barns
carriage house	other _____				

Landscape Features: gardens pond ☒ mature trees slate sidewalks  
fountain hitching post carriage steps walls historic marker (describe narrative)  
well mile post monument/sculpture other \_\_\_\_\_

Prepared by: Walter Richard Wheeler Address 1744 Washington Avenue Extension, Rensselaer, NY 12144

Telephone: 518-283-0534 Email [wwheeler@hartgen.com](mailto:wwheeler@hartgen.com) Date 29 April 2014

PIN: 1757.32 Organization: Hartgen Archeological Associates, Inc.

(See Reverse)



<u>Building Surroundings:</u>	open land	woodland	scattered buildings	densely built-up
commercial	industrial	■ residential	agricultural	
other _____				

**Photos:**



Photo 69. View looking north-northwest at 22 West Poplar Drive.





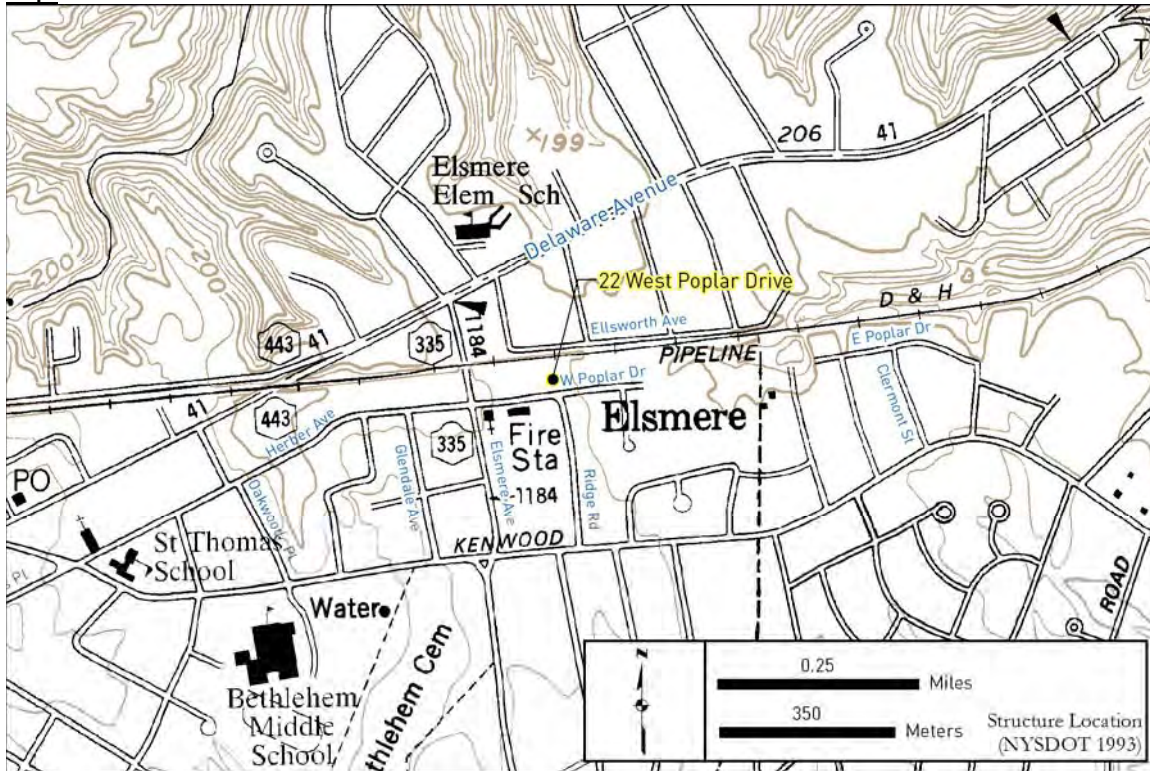
Photo 70. View looking north-northeast at 22 West Poplar Drive.



Photo 71. View looking south at 22 West Poplar Drive, with detached garage at center left.



**Map:**



**Narrative Description of Property:**

A bungalow style one-and-one-half story wood-framed house on concrete block foundation, this dwelling is sheathed with clapboards. It has a cross-gable roof, the south (street) elevation of which is supported on two square columns supported on truncated pyramidal plinths, sheltering a porch. The T-shaped plan has a center passage; the entrance is sheltered by the porch and is flanked by paired windows. The entry steps are flanked by pyramidal plinths of the same design as those supporting the end columns, but terminate in stepped finials, rather than columns. The broad expanse of the porch is visually supported by a stepped beam. The north end of the cross-gable roof is articulated as a large gabled dormer. Most of the sash are 3-over-1 double hung, and are original. Smaller sash are 1-over-1, and also appear to be original. A small one-story gabled wing projects from the west end of the north elevation. The house appears to retain all of its original details and features.

A detached wood-frame garage is contemporary with the house, and detailed similar to it. It is intact to its period of construction, except for the replacement of what were probably two pairs of carriage doors with a single large pull up door.

**Narrative Description of National Register Eligibility**

An intact representative example of a bungalow style dwelling constructed in the interwar years (1918-1941), this house is eligible for listing on the National Register under Criterion C.

**Sources:**

Hubka, Thomas C.

2013 *Houses Without Names: Architectural Nomenclature and the Classification of America's Common Houses.*  
Knoxville, TN: University of Tennessee Press.

Howe, Jeffery, ed.

2002 *The Houses We Live In: An Identification Guide to the History and Style of American Domestic Architecture.*  
London: PRC Publishing Company, Ltd.





NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY FORM

DATE: 25 April 2014 PIN: 1757.32 BIN: none known

PREPARER/ AFFILIATION: Walter R Wheeler, Hartgen Archeological Associates

EVALUATION APPLYING METHODOLOGY OF NYSDOT 2002 HISTORIC BRIDGE INVENTORY

National Register Eligible Yes National Register Criteria C Not Eligible     

IDENTIFICATION

1. BRIDGE NAME(S): (if known) Cattle Pass [ structure 63]

2. TOWN/CITY/VILLAGE (MCD): Bethlehem HAMLET:     

3. COUNTY: Albany

4. FEATURE CARRIED (street, route no., railroad): originally Albany and Susquehanna Railroad, later CSX

5. FEATURE CROSSED (river, highway, railroad):     

6. YEAR BUILT: c. 1890

DESCRIPTION

7. BRIDGE TYPE: cattle Pass

7a. Number of Spans: one 7b. Length of Span(s): about 15 feet

8. STRUCTURAL MATERIAL: a. timber X b. stone X c. steel      d. concrete      e. cast/ wrought iron      f. other     

8a. Abutment Material: concrete      stone faced      laid-up stone X other     

9. PHOTOS: (see attached)



Photo 72. View looking northeast at the cattle pass.





Photo 73. View northeast, showing typical timber and stone abutment detail.



Photo 74. View southeast, showing underside of wood deck and abutment detail.



10. INTEGRITY: a. list major alterations and dates (if known): \_\_\_\_\_  
b. previous use cattle pass for railroad c. moved \_\_\_\_\_ if so, when? \_\_\_\_\_

11. RELATED BUILDINGS AND PROPERTY (check more than one if necessary): a. power house \_\_\_\_\_ b. railroad station \_\_\_\_\_  
c. bridge operators house \_\_\_\_\_ d. landscape features (specify) (i.e. stone walls, light standards) \_\_\_\_\_  
e. other \_\_\_\_\_

12. BRIDGE SURROUNDINGS (check more than one if necessary): a. open land \_\_\_\_\_ b. woodland ☒ c. scattered buildings \_\_\_\_\_  
d. densely built-up \_\_\_\_\_ e. commercial \_\_\_\_\_ f. industrial \_\_\_\_\_ g. residential \_\_\_\_\_ h. potentially eligible historic district \_\_\_\_\_ i. other \_\_\_\_\_

13. OTHER NOTABLE BRIDGE FEATURES (e.g. aesthetic treatment, multiple spans, cantilevered):

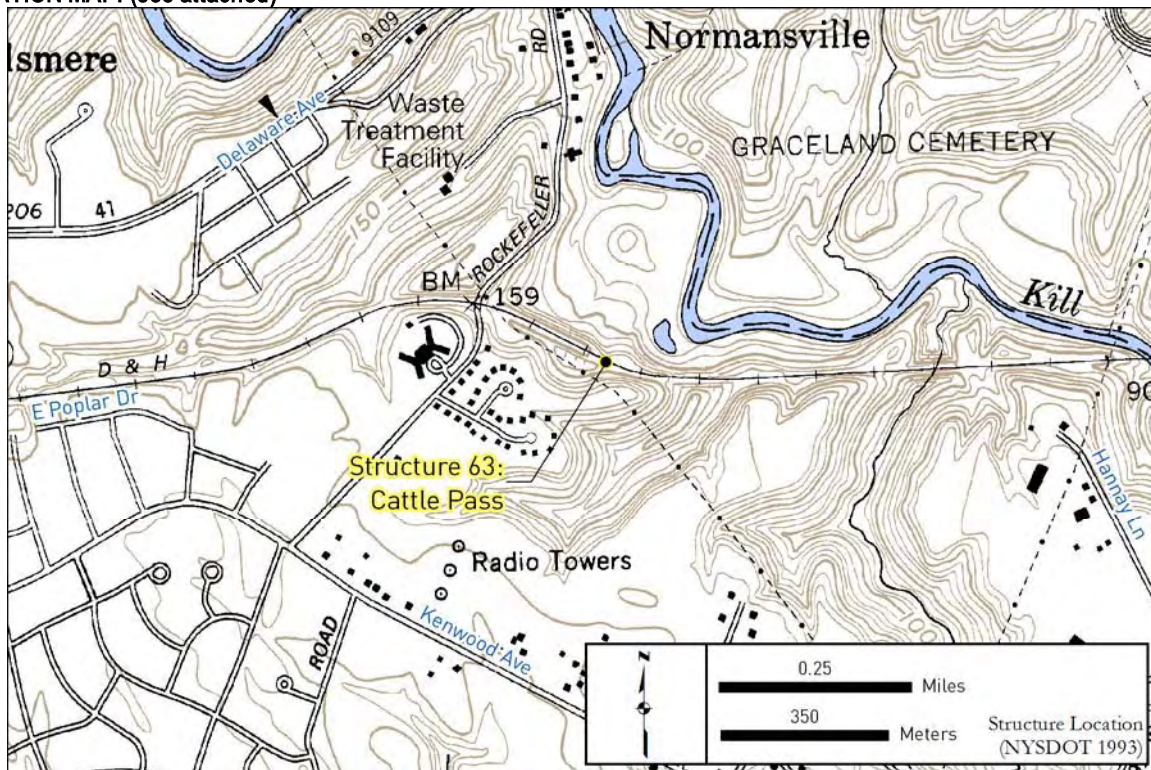
Rusticated limestone abutments support this single span bridge of diminutive size. The deck and portions of the abutments are comprised of large timbers.

14. HISTORIC IMPORTANCE/ ASSOCIATION (include plate information): Engineer or builder:

This cattle pass was constructed as an early or original feature of the Albany and Susquehanna Railroad. This section of the railroad dates to the early 1860s; while it is possible that this feature was built at that time, the stonework appears to be more typical of features contemporary to the later nineteenth century.

Several culverts and cattle passes are located along this segment of the rail line. These are an early or original feature associated with the construction of the berm which the tracks pass over. In a 1906 inspection report they were described as "open culverts and cattle passes [which] have I-beam or rail stringers and all have standard floors, in good repair. A few timber stringers yet remain on the...Albany branch..." (Dunn 1907:342-43). This may be one of the "few timber stringer" cattle passes described in that report.

15. LOCATION MAP: (see attached)







NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY FORM

DATE: 25 April 2014 PIN: 1757.32 BIN: none known

PREPARER/ AFFILIATION: Walter Richard Wheeler, Hartgen Archeological Associates Inc.

EVALUATION APPLYING METHODOLOGY OF NYSDOT 2002 HISTORIC BRIDGE INVENTORY

National Register Eligible Yes National Register Criteria C Not Eligible     

IDENTIFICATION

1. BRIDGE NAME(S): (if known) culvert with stone walls [ structure 67]

2. TOWN/CITY/VILLAGE (MCD): Delmar HAMLET:                     

3. COUNTY: Albany

4. FEATURE CARRIED (street, route no., railroad): originally Albany and Susquehanna Railroad, later CSX

5. FEATURE CROSSED (river, highway, railroad):   

6. YEAR BUILT: c.1890

DESCRIPTION

7. BRIDGE TYPE: culvert/cattle pass with stone wall abutments

7a. Number of Spans: one 7b. Length of Span(s): approximately 10 feet

8. STRUCTURAL MATERIAL: a. timber      b. stone      c. steel X d. concrete      e. cast/ wrought iron X f. other     

8a. Abutment Material: concrete      stone faced      laid-up stone X other                     

9. PHOTOS:





Photo 75. View looking southwest at culvert/cattle pass.



Photo 76. View northwest toward culvert/cattle pass.



Photo 77. View north showing details of abutments and steel rail-supported deck.





Photo 78. Detail showing section of rails used for decking.

10. INTEGRITY: a. list major alterations and dates (if known): none known  
 b. previous use cattle pass c. moved if so, when?

11. RELATED BUILDINGS AND PROPERTY (check more than one if necessary): a. power house    b. railroad station     
 c. bridge operators house    d. landscape features (specify) (i.e. stone walls, light standards)     
 e. other   

12. BRIDGE SURROUNDINGS (check more than one if necessary): a. open land    b. woodland X c. scattered buildings     
 d. densely built-up    e. commercial    f. industrial    g. residential    h. potentially eligible historic district    i. other   

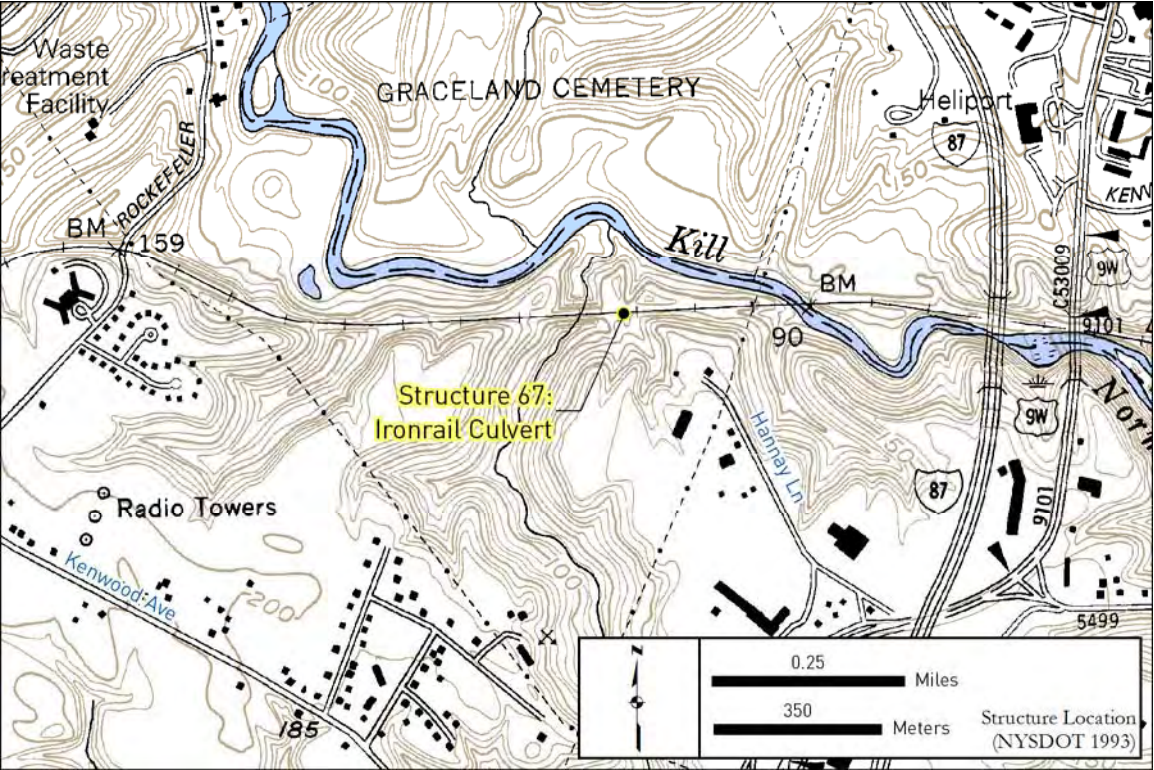
13. OTHER NOTABLE BRIDGE FEATURES (e.g. aesthetic treatment, multiple spans, cantilevered):  
 Rusticated limestone abutments; iron rails support deck.

14. HISTORIC IMPORTANCE/ ASSOCIATION (include plate information): **Engineer or builder:**

This cattle pass/culvert may be one of the original features of the Albany and Susquehanna Railroad, which was originally constructed in the early 1860s. This feature may date to the later nineteenth century; the stonework of the abutments is similar to that used by railroads at that time. An inspection report from 1906 describes this and identical nearby structures as "open culverts and cattle passes [which] have I-beam or rail stringers and all have standard floors, in good repair." (Dunn 1907:342-43). The structure remains as described in the 1906 report.



15. LOCATION MAP:







NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY FORM

DATE: 25 April 2014 PIN: 1757.32 BIN: none known

PREPARER/ AFFILIATION: Walter Richard Wheeler, Hartgen Archeological Associates Inc.

EVALUATION APPLYING METHODOLOGY OF NYSDOT 2002 HISTORIC BRIDGE INVENTORY

National Register Eligible Yes National Register Criteria C Not Eligible     

IDENTIFICATION

1. BRIDGE NAME(S): (if known) cattle pass with stone abutments ( structure 69)
2. TOWN/CITY/VILLAGE (MCD): Albany HAMLET:
3. COUNTY: Albany
4. FEATURE CARRIED (street, route no., railroad): originally Albany and Susquehanna Railroad, later CSX
5. FEATURE CROSSED (river, highway, railroad):
6. YEAR BUILT: c. 1890

DESCRIPTION

7. BRIDGE TYPE: small bridge with stone abutments
- 7a. Number of Spans: one 7b. Length of Span(s): approximately six feet
8. STRUCTURAL MATERIAL: a. timber      b. stone X c. steel X d. concrete      e. cast/ wrought iron      f. other
- 8a. Abutment Material: concrete      stone faced      laid-up stone X other

9. PHOTOS:



Photo 79. General view looking northwest.





Photo 80. View looking southwest.

10. INTEGRITY: a. list major alterations and dates (if known): \_\_\_\_\_  
 b. previous use cattle pass c. moved \_\_\_\_\_ if so, when? \_\_\_\_\_

11. RELATED BUILDINGS AND PROPERTY (check more than one if necessary): a. power house \_\_\_ b. railroad station \_\_\_  
 c. bridge operators house \_\_\_ d. landscape features (specify) (i.e. stone walls, light standards) \_\_\_\_\_  
 e. other \_\_\_\_\_

12. BRIDGE SURROUNDINGS (check more than one if necessary): a. open land \_\_\_ b. woodland ☒ c. scattered buildings \_\_\_  
 d. densely built-up \_\_\_ e. commercial \_\_\_ f. industrial \_\_\_ g. residential \_\_\_ h. potentially eligible historic district \_\_\_ i. other \_\_\_

13. OTHER NOTABLE BRIDGE FEATURES (e.g. aesthetic treatment, multiple spans, cantilevered):

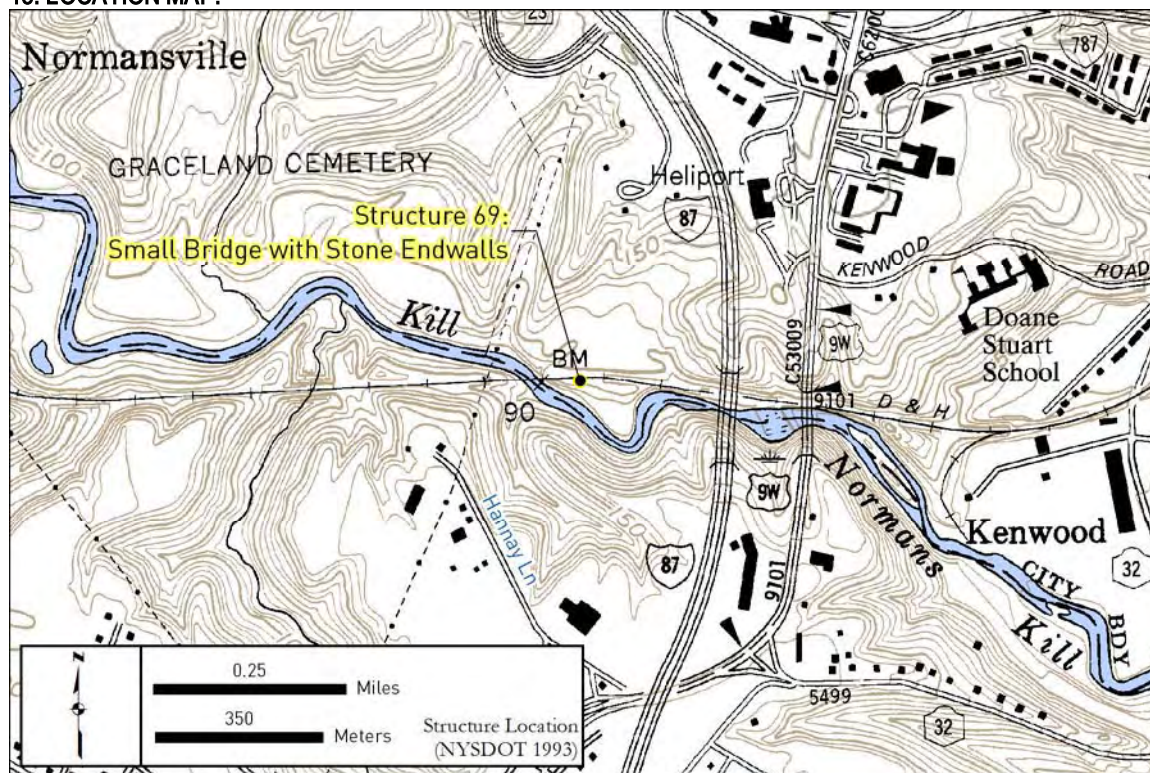
Rusticated limestone abutments; steel or iron rails support deck.

14. HISTORIC IMPORTANCE/ ASSOCIATION (include plate information): Engineer or builder:

This cattle pass/culvert may be one of the original features of the Albany and Susquehanna Railroad, which was originally constructed in the early 1860s. This feature may date to the later nineteenth century; the stonework of the abutments is similar to that used by railroads at that time. An inspection report from 1906 describes this and identical nearby structures as "open culverts and cattle passes [which] have I-beam or rail stringers and all have standard floors, in good repair." (Dunn 1907:342-43). The structure remains as described in the 1906 report.



15. LOCATION MAP:







NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY FORM

DATE: 29 April 2014 PIN: 1757.32 BIN: 5512549 9 (southbound)  
PREPARER/ AFFILIATION: Walter Richard Wheeler, Hartgen Archeological Associates Inc.  
EVALUATION APPLYING METHODOLOGY OF NYSDOT 2002 HISTORIC BRIDGE INVENTORY  
National Register Eligible Yes National Register Criteria A and C Not Eligible     

IDENTIFICATION

1. BRIDGE NAME(S): (if known) NYS Thruway Bridge (southbound) [ structure 71]  
2. TOWN/CITY/VILLAGE (MCD): Albany HAMLET:                       
3. COUNTY: Albany  
4. FEATURE CARRIED (street, route no., railroad): I-87, southbound  
5. FEATURE CROSSED (river, highway, railroad): Normans Kill  
6. YEAR BUILT: 1954

DESCRIPTION

7. BRIDGE TYPE: continuous truss deck  
7a. Number of Spans: 3 7b. Length of Span(s): main span approx. 240 feet; total length: 615 feet  
8. STRUCTURAL MATERIAL: a. timber      b. stone      c. steel X d. concrete      e. cast/ wrought iron      f. other       
8a. Abutment Material: concrete X stone faced      laid-up stone      other                       
9. PHOTOS:



Photo 81. View east-southeast showing the main span.





Photo 82. View south showing the abutment and southern span (at right).



Photo 83. View looking north-northwest, showing north abutment, at left.

10. INTEGRITY: a. list major alterations and dates (if known): rehabilitated 1998  
b. previous use \_\_\_\_\_ c. moved    if so, when? \_\_\_\_\_

11. RELATED BUILDINGS AND PROPERTY (check more than one if necessary): a. power house    b. railroad station     
c. bridge operators house    d. landscape features (specify) (i.e. stone walls, light standards) \_\_\_\_\_  
e. other \_\_\_\_\_

12. BRIDGE SURROUNDINGS (check more than one if necessary): a. open land    b. woodland   x   c. scattered buildings   x



d. densely built-up\_\_ e. commercial\_\_ f. industrial\_\_ g. residential\_\_ h. potentially eligible historic district\_\_ i. other\_\_

**13. OTHER NOTABLE BRIDGE FEATURES (e.g. aesthetic treatment, multiple spans, cantilevered):**

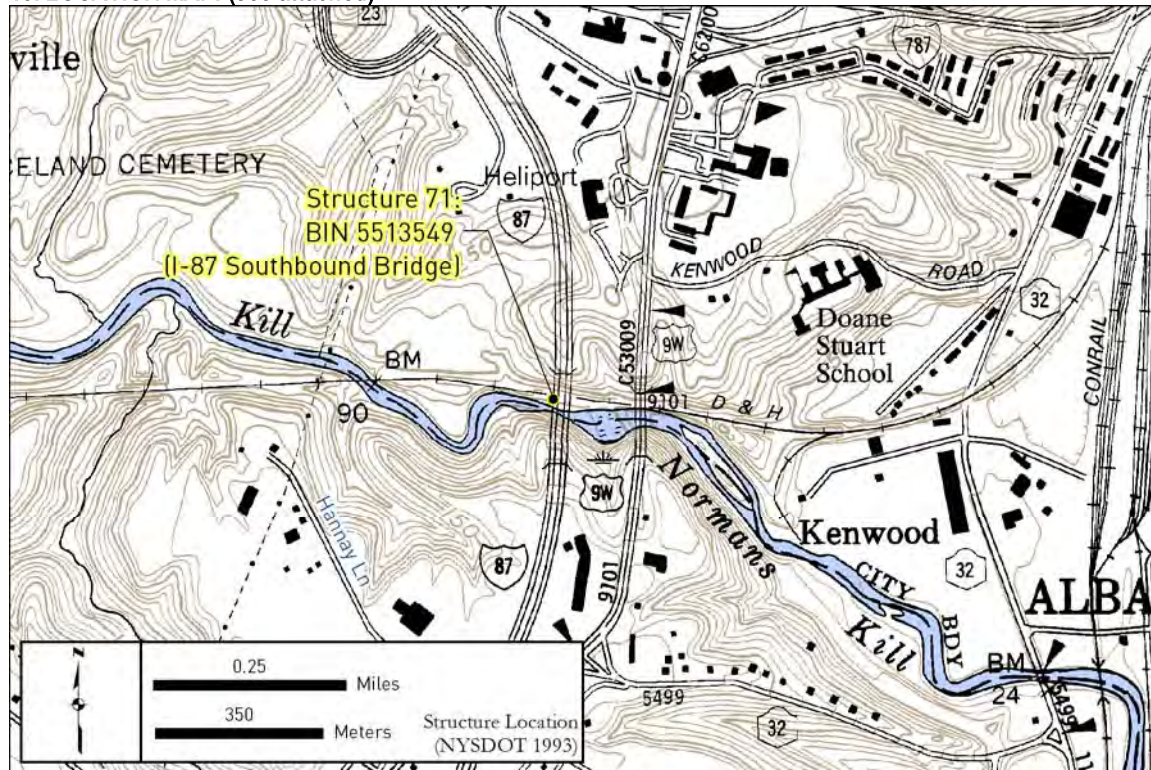
The choice of paired continuous truss deck bridges in this location appears to have been the result of aesthetic considerations. The curves of the bottoms of the end spans of each bridge follow closely the outlines of the two banks of the Normans Kill ravine.

**14. HISTORIC IMPORTANCE/ ASSOCIATION (include plate information): Engineer or builder:**

M. J. Madigan, consulting engineer, who was closely associated with many public projects initiated by Robert Moses [Christopher Innes. *Designing Modern America: Broadway to Main Street* (New Haven: Yale University Press, 2005), 181] . The I-87 Bridge over the Normans Kill is an original component of the New York State Thruway, among the largest public works in the nation when it was initiated in 1942. Construction began at the west end of the state in 1946, immediately after the close of World War II, and proceeded eastward and south, with the section carried by this bridge opening in 1954 (<http://www.upstatenyroads.com/thruway-history1.shtml> , accessed 29 April 2014). One of two near-identical spans, located immediately adjacent to each other; the northbound bridge is located immediately to the east.

The construction of the thruway was closely connected to war-preparedness; a number of its lengths were designed to be able to act as emergency air strips.

**15. LOCATION MAP: (see attached)**







NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY FORM

DATE: 29 April 2014 PIN: 1757.32 BIN: 5512549 9 (northbound)

PREPARER/ AFFILIATION: Walter Richard Wheeler, Hartgen Archeological Associates Inc.

EVALUATION APPLYING METHODOLOGY OF NYSDOT 2002 HISTORIC BRIDGE INVENTORY

National Register Eligible Yes National Register Criteria A and C Not Eligible     

IDENTIFICATION

1. BRIDGE NAME(S): (if known) NYS Thruway Bridge (northbound) [ structure 72]

2. TOWN/CITY/VILLAGE (MCD): Albany HAMLET:     

3. COUNTY: Albany

4. FEATURE CARRIED (street, route no., railroad): I-87 (northbound)

5. FEATURE CROSSED (river, highway, railroad): Normans Kill

6. YEAR BUILT: 1954

DESCRIPTION

7. BRIDGE TYPE: continuous truss deck

7a. Number of Spans: 3 7b. Length of Span(s): main span approx. 240 feet; total length: 615 feet

8. STRUCTURAL MATERIAL: a. timber      b. stone      c. steel X d. concrete      e. cast/ wrought iron      f. other     

8a. Abutment Material: concrete X stone faced      laid-up stone      other     

9. PHOTOS:



Photo 84. View west looking at bridge.





Photo 85. View southwest, showing south abutment at left center. Contemporary southbound bridge is at right.



Photo 86. view west-northwest, showing north abutment.

10. INTEGRITY: a. list major alterations and dates (if known): rehabilitated 1998  
 b. previous use \_\_\_\_\_ c. moved \_\_\_\_\_ if so, when? \_\_\_\_\_

11. RELATED BUILDINGS AND PROPERTY (check more than one if necessary): a. power house \_\_\_\_\_ b. railroad station \_\_\_\_\_  
 c. bridge operators house \_\_\_\_\_ d. landscape features (specify) (i.e. stone walls, light standards) \_\_\_\_\_  
 e. other \_\_\_\_\_

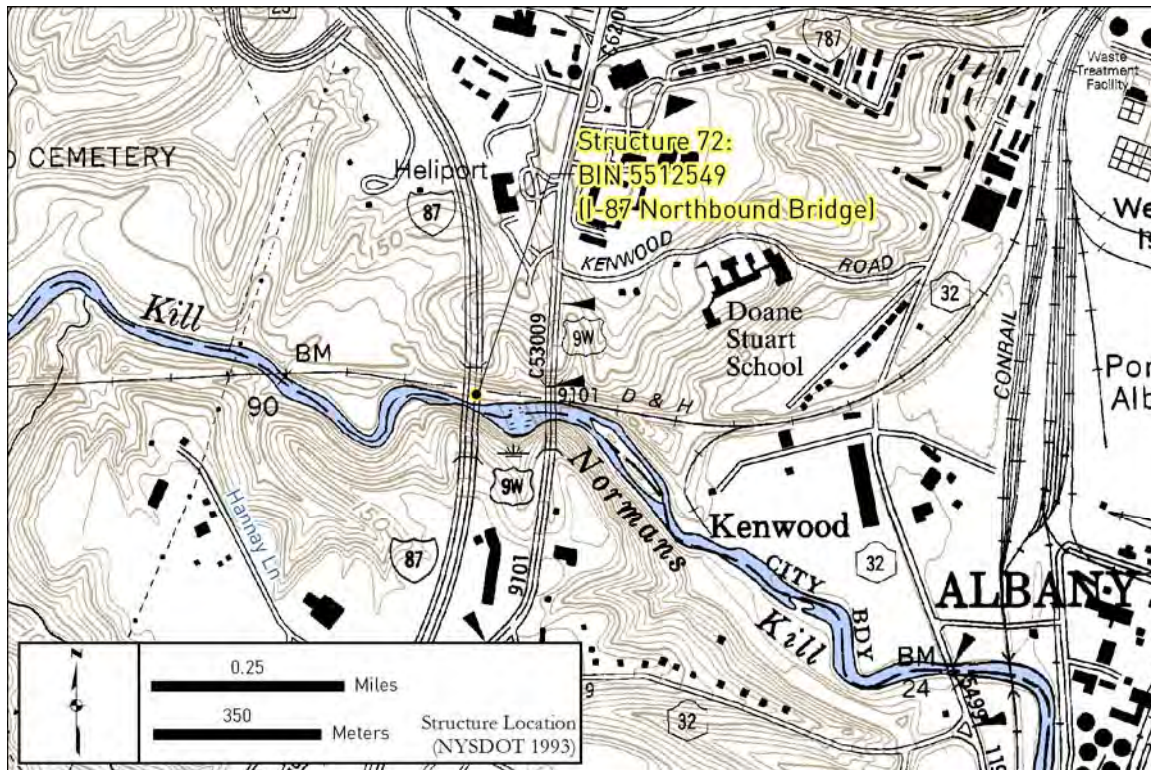
12. BRIDGE SURROUNDINGS (check more than one if necessary): a. open land \_\_\_\_\_ b. woodland X c. scattered buildings X  
 d. densely built-up \_\_\_\_\_ e. commercial \_\_\_\_\_ f. industrial \_\_\_\_\_ g. residential \_\_\_\_\_ h. potentially eligible historic district \_\_\_\_\_ i. other \_\_\_\_\_



The choice of paired continuous truss deck bridges in this location appears to have been the result of aesthetic considerations. The curves of the bottoms of the end spans of each bridge follow closely the outlines of the two banks of the Normans Kill ravine.

M. J. Madigan, consulting engineer, who was closely associated with many public projects initiated by Robert Moses [Christopher Innes. *Designing Modern America: Broadway to Main Street* (New Haven: Yale University Press, 2005), 181]. The I-87 Bridge over the Normans Kill is an original component of the New York State Thruway, among the largest public works in the nation when it was initiated in 1942. Construction began at the west end of the state in 1946, immediately after the close of World War II, and proceeded eastward and south, with the section carried by this bridge opening in 1954 (<http://www.upstatenyroads.com/thruway-history1.shtml>, accessed 29 April 2014). One of two near-identical spans, located immediately adjacent to each other; the southbound bridge is located immediately to the west.

### 15. LOCATION MAP:







NEW YORK STATE DEPARTMENT OF TRANSPORTATION  
BRIDGE INVENTORY FORM

DATE: 25 April 2014 PIN: 1757.32 BIN: 7022460  
PREPARER/ AFFILIATION: Walter Richard Wheeler, Hartgen Archeological Associates Inc.  
EVALUATION APPLYING METHODOLOGY OF NYSDOT 2002 HISTORIC BRIDGE INVENTORY  
National Register Eligible Yes National Register Criteria C Not Eligible     

**IDENTIFICATION**

1. BRIDGE NAME(S): (if known) RR Bridge over South Pearl Street (structure 91)  
2. TOWN/CITY/VILLAGE (MCD): Albany HAMLET:                       
3. COUNTY: Albany  
4. FEATURE CARRIED (street, route no., railroad): originally Delaware & Hudson Railroad, later CSX  
5. FEATURE CROSSED (river, highway, railroad): South Pearl Street  
6. YEAR BUILT: 1932

**DESCRIPTION**

7. BRIDGE TYPE: Thru Plate Girder Bridge  
7a. Number of Spans: one 7b. Length of Span(s): 43 feet  
8. STRUCTURAL MATERIAL: a. timber      b. stone      c. steel X d. concrete      e. cast/ wrought iron      f. other       
8a. Abutment Material: concrete X stone faced      laid-up stone      other                       
9. PHOTOS:



Photo 87. View north from South Pearl Street.





Photo 88. View west at deck level.



Photo 89. View northeast showing east abutment and underside of deck.





Photo 90. Bridge plaque, mounted to the east end of the north plate girder.

10. INTEGRITY: a. list major alterations and dates (if known): none known  
 b. previous use railroad overpass c. moved      if so, when?                     

11. RELATED BUILDINGS AND PROPERTY (check more than one if necessary): a. power house      b. railroad station       
 c. bridge operators house      d. landscape features (specify) (i.e. stone walls, light standards)                       
 e. other                     

12. BRIDGE SURROUNDINGS (check more than one if necessary): a. open land      b. woodland      c. scattered buildings       
 d. densely built-up      e. commercial      f. industrial ☒ g. residential      h. potentially eligible historic district      i. other     

13. OTHER NOTABLE BRIDGE FEATURES (e.g. aesthetic treatment, multiple spans, cantilevered):

Abutments are board-formed concrete.

14. HISTORIC IMPORTANCE/ ASSOCIATION (include plate information): Engineer or builder:

Constructed for use by the D&H railroad, this bridge was necessitated by alterations to Route 32, which was moved in the early 1930s to a point east of its original course. Builder's plate reads "Built by/ McClintic-Marshall/Corporation/1932" (Photo 90). This bridge is nearly identical to BIN 7046170, located on the same line and constructed at about the same time; that bridge was determined by a previous survey to be eligible for the National Register.



15. LOCATION MAP:

